



**SW Admiral Way Bridges
Replacement Planning Study
Alternatives Comparison Report**



Seattle
Department of
Transportation

May 31, 2024



SW Admiral Way Bridges
Replacement Planning Study
Alternatives Comparison Report

Client PM: Sara Zora
Version: Final
Date: May 31, 2024

Project no: W3X88300
Project manager: Mark Johnson, PE
Prepared by: Adrian Corella, PE

Document history and status:

Version	Date	Description	Author	Reviewed	Approved
1	12/15/23	Draft Report	AC	MJ	MJ
2	5/10/24	Pre-Final Report	AC	MJ	MJ
3	5/31/24	Final Report	AC	MJ	MJ

Jacobs Engineering Inc.
1100 112th Avenue NE
Suite 500
Bellevue, WA 98004-5118
425-453-5000

Contents

Executive Summary	iv
1. Introduction	1
1.1 Study Purpose	1
1.2 Project Background	3
1.3 Evaluation Process.....	3
2. Site Conditions	4
2.1 Right-of-way	4
2.2 Complete Streets Evaluation	6
2.2.1 Existing Conditions.....	6
2.2.2 Bridge Cross Sections within the Existing Footprint.....	7
2.2.3 Expanded Width	10
2.3 Traffic Mobility	11
2.4 Stormwater Management.....	14
2.4 Utilities	15
2.5 Geotechnical Conditions	15
2.5.1 Subsurface Conditions	15
2.5.2 Slope Stability	16
2.5.3 Recommended Future Geotechnical Explorations.....	16
3. Alternatives Considered	17
3.1 Alternative 1 – Bridge Replacement with Single Stage Construction	19
3.2 Alternative 2 – Bridge Replacement with Staged Construction	20
3.3 Alternative 3 – Bridge Rehabilitation	21
4. Alternatives Analysis Methodology and Evaluation	25
4.1 Cost Considerations.....	26
4.2 Implementation Characteristics	27
4.3 Mobility and Connectivity	27
4.4 Environmental and Permitting Considerations.....	28
4.5 Stakeholder Input	28
4.6 Assessment Results and Sensitivity Analysis	30
5. Conclusions	33
5.1 Key Findings	33
5.2 Recommendations.....	34

Appendices

- Appendix A. Complete Streets Checklist
- Appendix B. Traffic Analysis Memorandum
- Appendix C. Stormwater Management Flowchart
- Appendix D. Concept-Level Cost Estimate
- Appendix E. Constructability Evaluation
- Appendix F. Construction Schedule
- Appendix G. Conceptual Bridge Replacement Plans
- Appendix H. Right-of-Way Assessment on SW Admiral Way
- Appendix I. Public Outreach Survey Results

Tables

- Table 3.1 FHWA-Recommended Service Life Categories
- Table 3.2 FHWA-Recommended Performance Levels
- Table 4.1 Evaluation Criteria with Weighting of Categories
- Table 4.2 Alternatives Screening Matrix
- Table 4.3 Results of Sensitivity Analysis

Figures

- Figure 1.1 Project Vicinity Map
- Figure 1.2 Opportunities and Constraints Map
- Figure 2.1 Aerial View of Bridges Showing Parcel Lines
- Figure 2.2 West End of Bridge – SW Admiral Way
- Figure 2.3 East End of Bridge – SW Admiral Way
- Figure 2.4 Existing Bridges (Looking West)
- Figure 2.5 Existing Cross Section of Admiral Way Bridges
- Figure 2.6 Minimal Improvements, 64' wide
- Figure 2.7 Bike and Pedestrian-Oriented Improvements, 64' wide
- Figure 2.8 All Modes Accommodated, 64' wide
- Figure 2.9 Road Rechannalization, 64' wide
- Figure 2.10 Compromise Cross Section, 64' wide
- Figure 2.11 Expanded Bridge, 72' wide
- Figure 2.12 Expanded Bridge, 80' wide
- Figure 2.13 SW Admiral Way Detour Routes
- Figure 2.14 Fairmount Avenue SW Detour Route
- Figure 2.15 Seattle City Light Lines Map
- Figure 2.16 Elevation View of North Bridge
- Figure 3.1 Elevation View of Proposed Replacement Bridge
- Figure 3.2 Bicycle/Pedestrian Routes Near SW Admiral Way
- Figure 3.3 Alternative 1 – Single Stage Construction
- Figure 3.4 Alternative 2 – Staged Bridge Construction
- Figure 3.5 Locations of Deterioration of North Bridge
- Figure 3.6 Critical Elements of Truss Under Vehicle Loading at North Bridge
- Figure 3.7 Concrete Corbel at End of Concrete Truss
- Figure 4.1 Evaluation Categories
- Figure 4.2 Survey Responses – Top Priorities for a Replacement Bridge
- Figure 4.3 Survey Responses – Preferred Construction Timeline

Executive Summary

The Admiral Way Bridges on SW Admiral Way carry more than 25,000 vehicles per day across the Fairmount Avenue SW ravine and are on one of the major east-west corridors of traffic into and out of the neighborhood of West Seattle. SW Admiral Way is classified as an Urban Center Connector and serves as a Minor Transit Route and a Minor Truck Street for freight.

While the bridges are kept safe for daily use, portions are approaching their 100th year of service, are showing signs of deterioration, and are deemed to be functionally obsolete. The Seattle Department of Transportation (SDOT) is seismically retrofitting the structures to improve the seismic performance but anticipates that maintenance costs will continue to rise.

The purpose of this planning study is to explore feasible rehabilitation and replacement options and to develop costs and trade-offs of alternatives. The overall goal is to provide planning-level costs and impacts that can be used to plan for future replacement of the bridges.

The alternatives that were considered include:

- **Bridge Replacement Constructed in a Single Stage:** Remove and replace the existing bridges with a new bridge under complete closure of SW Admiral Way during construction.
- **Bridge Replacement Constructed in Two Stages:** Remove and replace the existing bridges with a new bridge allowing limited traffic on SW Admiral Way during construction.
- **Bridge Rehabilitation:** Leave the existing bridges in service and perform ongoing maintenance and rehabilitation to extend the service life.

The approach to evaluating alternatives focused on attributes in the following five broad categories:

- **Cost Considerations** that include planning-level estimates of construction, maintenance and operations, and right-of-way costs.
- **Implementation Characteristics** that consist of constructability, construction duration, public safety, and maintenance of traffic during construction.
- **Mobility and Connectivity** that include connectivity and access for vehicles, freight, and transit as well as people who walk and bike.
- **Environmental and Permitting Considerations** that include impacts to adjacent land use and Environmentally Critical Areas, visual impacts, property acquisition, and historic preservation.
- **Stakeholder Input** from the West Seattle community, nearby residents, local businesses, other agencies, and the Cascade Bicycle Club.

Based on a study of alternatives, the key advantages and disadvantages of each alternative are as follows:

Bridge Replacement, Single Stage Construction

- **Advantages:** Lower estimated construction cost; Shorter construction duration leading to shorter impacts to the traveling public; More opportunities for creative means and methods of construction; Higher public safety due to minimized interaction with construction activities.

- **Disadvantages:** Significant disruption to traffic flow requiring extensive detour planning; Substantial impact on local community and businesses during construction; Removal of a National Register of Historic Places (NRHP) eligible bridge; Would not meet SEPA exemption criteria.

Bridge Replacement, Staged Bridge Construction

- **Advantages:** Maintain partial traffic flow, reducing the need for detours; Less direct impact on the local community; Allow for gradual adaptation to construction activities by motorists.
- **Disadvantages:** Higher estimated construction cost; Prolonged construction period with extended community exposure to construction; Potential for construction fatigue among local residents and businesses; Reduced public safety to the traveling public; Removal of a National Register of Historic Places (NRHP) eligible bridge; Would not meet SEPA exemption criteria.

Rehabilitation of Existing Bridges

- **Advantages:** Lowest initial financial outlay; Minimal disruption to traffic during repair work; Maintain current traffic patterns and connectivity; Lower adverse effect to a National Register of Historic Places (NRHP) eligible bridge; Would likely meet SEPA exemption criteria.
- **Disadvantages:** Potential for higher long-term maintenance costs; May not fully address all the functionally obsolete elements to current design standards; Likelihood of recurring disruptions for ongoing maintenance.

Based on the screening of alternatives, bridge replacement in a single stage of construction consistently scored highest in consideration of the evaluation criteria, with the primary challenge being the management of traffic disruptions during construction. The staged construction approach, while more costly and prolonged, offers a balance between the efficiency of construction and maintenance of traffic. The estimated project cost of the single-stage bridge replacement is between \$22.8 and \$42.8 million in 2024 dollars, whereas the estimated cost of the staged bridge replacement is between \$29.8 and \$55.8 million.

Bridge rehabilitation may not fully address all the functionally obsolete elements to current design standards. In addition, given the advanced age of the bridges beyond their service life, the cost to maintain the bridges is expected to increase significantly over time, eventually making ongoing maintenance and repair impractical from a cost/benefit standpoint. This alternative is not recommended.

The final selection of a preferred alternative should balance costs, operational efficiency, safety considerations, environmental impact, and community well-being, aiming to deliver a sustainable and long-lasting solution for the SW Admiral Way corridor in West Seattle.

1. Introduction

1.1 Study Purpose

The Admiral Way Bridges on the SW Admiral Way corridor carry more than 25,000 vehicles per day across the Fairmount Avenue SW ravine and are on one of the major east-west corridors of traffic into and out of the neighborhood of West Seattle. SW Admiral Way is classified as an Urban Center Connector and serves as a Minor Transit Route and a Minor Truck Street for freight. Figure 1.1 shows the location of the bridges.

While the bridges are kept safe for daily use, portions are approaching their 100th year of service, are showing signs of deterioration, and are deemed to be functionally obsolete. The Seattle Department of Transportation (SDOT) is seismically retrofitting the structures to improve the seismic performance but anticipates that maintenance costs will continue to rise for these structures.

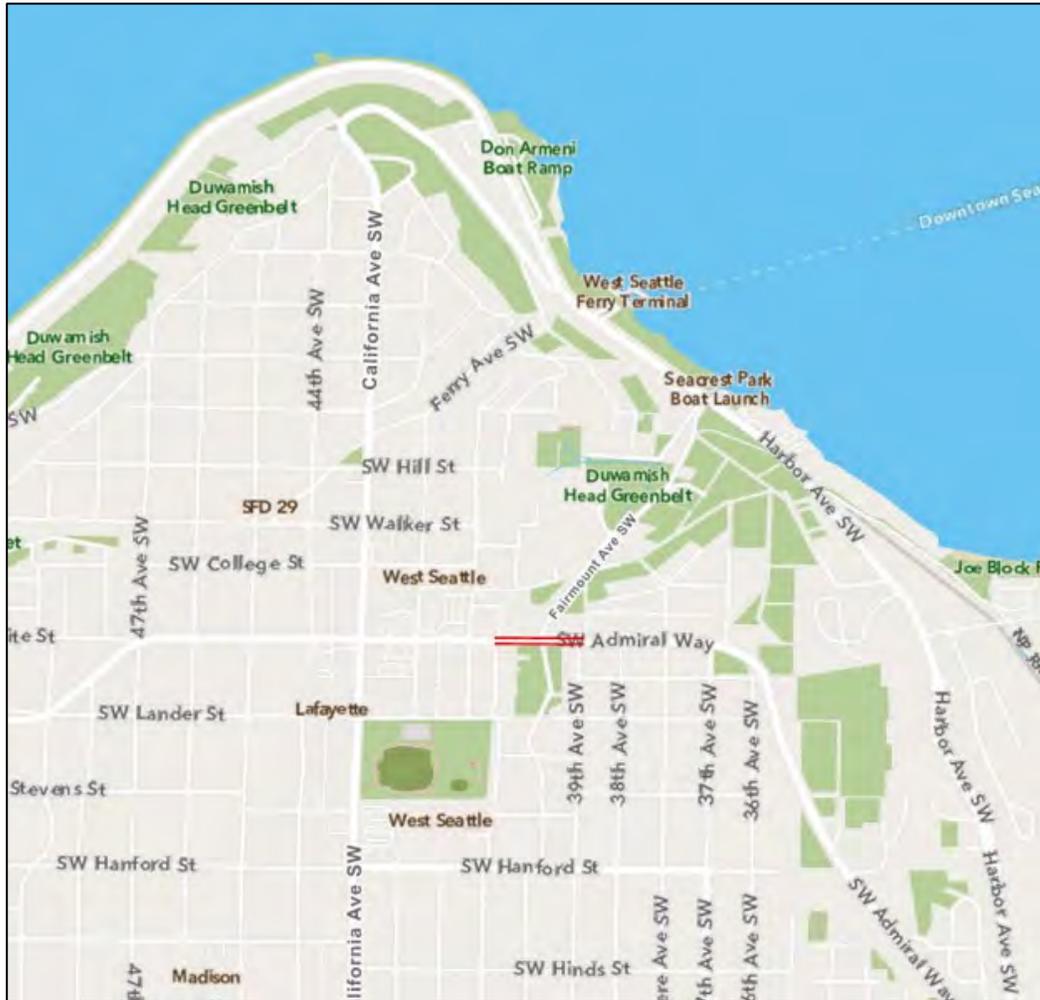


Figure 1.1 Project Vicinity Map

As part of the Levy to Move Seattle, the City funded this study to develop and evaluate rehabilitation and replacement options considering the constraints and opportunities at the project site. This planning study is intended to provide:

- A comparison of feasible rehabilitation and replacement options that SDOT and elected officials can use for future replacement or rehabilitation of the bridges or when considering future studies.
- Assessment of bike and pedestrian safety improvements that could be incorporated into the bridges and the SW Admiral Way corridor.
- Assessment of construction options and their effect on all modes of transportation that use the SW Admiral Way corridor and/or Fairmount Ave SW.

Figure 1.2 identifies opportunities, constraints, and risks to consider as alternatives are evaluated.



Figure 1.2 Opportunities and Constraints Map

1.2 Project Background

The Admiral Way Bridges are located between Walnut Avenue SW and 39th Avenue SW in Seattle's North Admiral neighborhood. The bridges span a steep ravine that is traversed by Fairmount Ave SW. The two bridges consist of distinct structures built at different times that are connected at the roadway deck level. The total width of the bridges is 64 feet and they carry a four-lane, 48-foot roadway with 5-6-foot wide sidewalks on each side.

The North bridge was constructed in 1927 and is a 290-foot, seven-span reinforced concrete structure. The bridge is comprised of 63-foot long approach spans at each end of the bridge, an 80-foot concrete truss span in the center of the bridge and 15-foot long drop-in slabs spanning in between the truss and the approach spans. The truss span is supported by two tall reinforced concrete bents consisting of octagonal columns, horizontal beams, and diagonal braces. The approach spans are supported by reinforced concrete bents with octagonal columns and horizontal beams without diagonal bracing. The columns at each bent are supported on spread footings with concrete pedestals. The South bridge, built in 1949, is a five-span, 298-foot steel plate girder bridge. The substructure is composed of wide flange steel columns with concentrically braced frames. All bent columns are supported on spread footings with concrete pedestals.

The bridges were seismically retrofitted in the mid-1990s. As part of the retrofit, the bridges were connected to join the two concrete decks for the entire length of the bridges. Other measures included closing the transverse expansion joints of the South bridge, strengthening the South bridge deck and abutments, and strengthening the North bridge abutments. Based on the record drawings, it was also intended to connect the tall columns of the North bridge together with a shear wall and retrofit the footings, but these items were not implemented.

As part of the Phase III Bridge Seismic Retrofit Program funded by the Move Seattle Levy, a subsequent seismic retrofit project will strengthen the North bridge deck chord and deck joint between the bridges, strengthen various other structural elements on both bridges, and stabilize the slopes of the ravine using drilled shafts and soil nails. These retrofit measures will improve the seismic performance of the bridges but will not change their current condition state. Given the advanced age of the bridges beyond their service life, the costs to maintain the bridges are expected to increase dramatically over time, eventually making ongoing maintenance impractical from a cost/benefit standpoint.

1.3 Evaluation Process

The intent of this study is for the evaluation of alternatives to be grounded in data-driven analysis, ensuring a holistic and unbiased approach to decision-making. The process consists of a study of alternatives using evaluation criteria that evaluate constructability, cost, impacts on traffic and local communities, environmental considerations, and long-term maintenance. Alternatives are compared using a screening matrix with each of the criteria categories given a weighting. The alternatives analysis is presented in Section 4.

2. Site Conditions

2.1 Right-of-way

The Admiral Way Bridges span over the ravine within a right-of-way corridor that is 80 feet wide on the east side of the ravine and narrower on the west side, as shown in Figure 2.1 below. The footprint of the North bridge extends several feet into the southeast corner of the northwest parcel in the ravine (Migu Realty LLC) based on King County parcel records.

The right-of-way limits between Walnut Ave SW and 39th Ave SW trace back to Ordinance 29062 that was issued in 1912 when a public right-of-way corridor across the ravine was established by condemnation of several properties. A portion of the existing North bridge, constructed in 1927, extends slightly past the right-of-way limits established in the ordinance, as shown in Appendix G, Figure 1.

The right-of-way boundaries were evaluated by SPU as part of this planning study to investigate discrepancies at the southern limit of the parcels located north of the bridges from Ordinance 29062. A summary of this assessment is included in Appendix H. SDOT is considering legally changing the right-of-way boundary of the northwest parcel (Migu Realty LLC) to match the north edge of the bridges using a quiet title claim. This action would remove any encumbrances that could jeopardize federal funding for future replacement of the bridges. With this boundary revision, the width of the right-of-way corridor on the west side of the ravine would increase to approximately 72.5 feet, as shown below in Figure 2.1.



Figure 2.1 Aerial View of Bridges Showing Parcel Lines (Source: King County Parcel Viewer)

Given the 64-foot width of the existing bridges, there is limited space to replace the bridges with a wider structure within the existing right-of-way corridor. There are also physical constraints at the ends of the bridges that would inhibit widening the roadway corridor over the ravine. The properties located at the corners of the bridges, shown in Figures 2.2 and 2.3, are located close to the edge of public right-of-way.

An increase in bridge footprint could require partial or full right-of-way takes at the following parcels:

- Parcel 9275703315 – 2515 39th Ave SW (residential home, northeast of bridges)
- Parcel 9345900105 – 2603 39th Ave SW (residential home, southeast of bridges)
- Parcel 5019500130 – 2600 A, 2602 B, and 2604 C Walnut Ave SW (townhouses, southwest of bridges)
- Parcel 9275703365 – 4000 SW Admiral Way (dental office, northwest of bridges)



Figure 2.2 West End of Bridge – SW Admiral Way (Looking East Toward the Bridges)



Figure 2.3 East End of Bridge – SW Admiral Way (Looking West Toward the Bridges)

2.2 Complete Streets Evaluation

The Complete Streets Checklist is a tool that SDOT uses to collect information about the status of the street and surroundings to help identify improvements that balance the needs of all users and enable safe mobility for pedestrians, bicyclists, motorists, and public transportation users through the transportation network. Typical elements that make up a complete street include sidewalks, bicycle facilities, shared-use paths, designated bus lanes, accessible transit stops, and frequent and safe crossings for pedestrians. The Complete Streets Checklist is included in Appendix A.

The Admiral Way Bridges are an integral point of access to the northern end of the West Seattle peninsula for people biking, walking, and driving and serve as a connection for goods and freight. The 2024 Seattle Transportation Plan (STP) reflects the demand for space along this limited right-of-way corridor, in which the bridges are designated as a minor freight route and a future bike+ arterial route. SW Admiral Way is also being planned as a future RapidRide route on the Frequent Transit Network, a designated Transit Access Corridor that is also a priority for Vision Zero investments.

As part of this study, bridge cross sections were explored that could balance the different modal priorities and designations given to SW Admiral Way. Existing and future modal plans were evaluated and SDOT subject matter experts were engaged to inform the development of bridge replacement cross sections. Due to the narrow right-of-way corridor, some cross sections do not meet standards and would need deviations.

Additional considerations include the possible STP SW Admiral Way Multimodal Improvement project which includes potential scope items such as “a bike route for people of all ages and abilities” and “potential bus and freight only lanes”. Because the right-of-way corridor on SW Admiral Way is wider than the existing bridges, bridge cross sections larger than the existing bridge width of 64 feet were also explored. All the ideas below are envisioned to have a north sidewalk overlook, above the Fairmount Ave SW right-of-way, to allow people walking or rolling to stop for the views of downtown Seattle.

2.2.1 Existing Conditions

The current roadway and sidewalk configuration of the bridges is shown in Figure 2.4. The existing 64-foot cross sectional width of the bridges includes the following elements, as shown in Figure 2.5:

- 5' wide sidewalk on the northside (substandard)
- 6' wide sidewalk on the southside (substandard)
- 13' shoulder lanes (above standard)
- 10.5' center lanes (substandard)
- 2' painted and c-curb median between east and westbound traffic
- Concrete barriers between the sidewalk and shoulder lanes
- Painted sharrows in each direction



Figure 2.4 Existing Bridges (Looking West)



Figure 2.5 Existing Cross Section of Admiral Way Bridges (64-foot Wide)

2.2.2 Bridge Cross Sections within the Existing Footprint

It is not possible to accommodate designated space for all modal classifications from the Seattle Transportation Plan (STP) within the existing bridge footprint. The cross sections in Figures 2.6 – 2.10 consider different options for accommodating people walking, biking, and driving within the existing bridge footprint. Several of the options consolidate pedestrian access to one side of the bridge to create a shared multi-use path for people walking and biking.

At this planning level, the cross sections do not consider shy distances to vertical barriers and show nominal widths of travel lanes, sidewalks, and bike paths. As a result, some of the cross sections do not meet standards and would need deviations.

These layouts were reviewed by the SDOT bicycle, pedestrian, and transit stakeholder teams. The following feedback was obtained:

- The SDOT bicycle and pedestrian teams expressed concern about a multi-use path as not ideal for either mode or transitioning back to the SW Admiral Way corridor directionally.
- The SDOT transit team supports the addition of bus lanes in the future when more frequent buses travel, although they are concerned about relatively low frequencies (1 bus every 10 minutes) leading to an often-empty bus lane currently. The transit team supports the idea of a shared bus/bike lane to increase the usability of the space but believes that a transit-only lane in the future may be necessary.

Figure 2.10 presents a compromise cross-section that increases access for all users while accepting the limitations of the right-of-way corridor width by matching the width of the existing bridges.

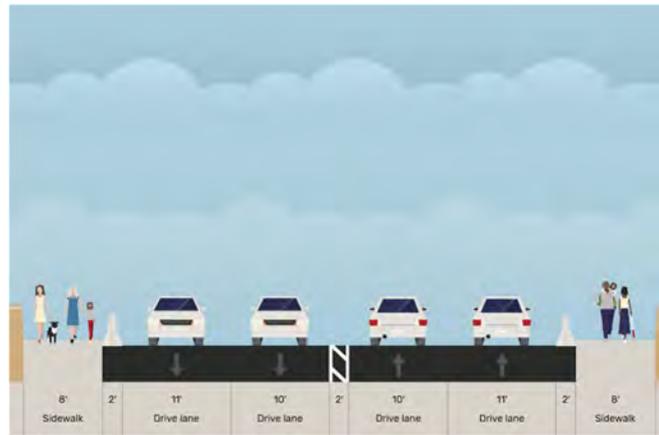


Figure 2.6 Minimal Improvements, 64' wide
(Brings widths to standard, does not include space for other modes)



Figure 2.7 Bike and Pedestrian-Oriented Improvements, 64' wide
(People walking and biking consolidated to north side with standard size travel lanes)



Figure 2.8 All Modes Accommodated, 64' wide
(People walking and biking consolidated to north side,
standard width lanes for bus and freight/general purpose lanes)



Figure 2.9 Road Rechannelization, 64' wide
(Above standard widths for bike lanes and sidewalks, only a single
travel lane in each direction with no dedicated transit lanes)

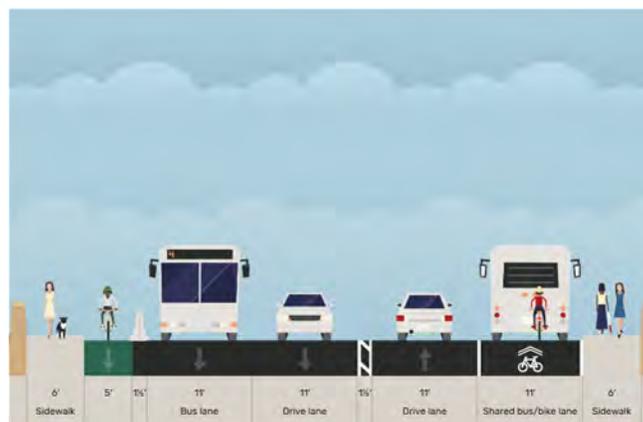


Figure 2.10 Compromise Cross Section, 64' wide
(Space for all modes, people biking downhill share eastbound bus lane)

2.2.3 Expanded Width

Although the right-of-way corridor on SW Admiral Way is constrained, bridge cross sections wider than the existing 64-foot width were also explored. A wider bridge would allow each mode to have its own dedicated space while also meeting Streets Illustrated standards.

To accommodate all Seattle Transportation Plan-designated modes (freight, transit, people biking, people walking) and meet standards, the bridge would need to be expanded to 72 feet, as shown in Figure 2.11. A bridge width of 80 feet (the maximum width of the right-of-way corridor) would allow oversized facilities for people walking and biking, as shown in Figure 2.12. However, given the narrower right-of-way corridor width on the west side of the ravine and the physical constraints at the ends of the bridges, widening the roadway across the ravine would likely require full or partial takes of some of the private properties at the ends of the bridges.

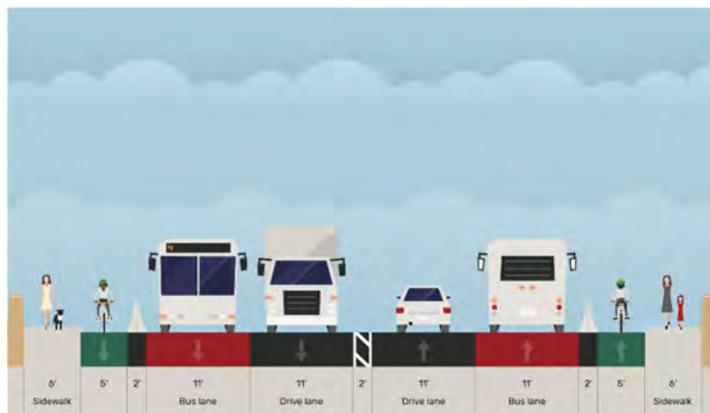


Figure 2.11 Expanded Bridge, 72' wide
(Allows for all modes to be accommodated to standard)

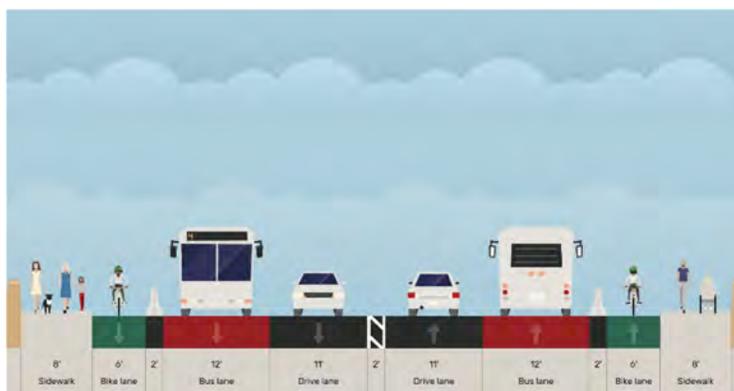


Figure 2.12 Expanded Bridge, 80' wide
(Maximum width within the right-of-way, allows comfortable and oversized facilities for people walking, rolling, and biking)

2.3 Traffic Mobility

The SW Admiral Way corridor carries more than 25,000 vehicles per day across the Fairmount Ave SW ravine and is a major east-west traffic corridor into and out of the neighborhood of West Seattle. The ravine extends for over half a mile and is traversed by Fairmount Ave SW, a two-lane road that extends north to Harbor Avenue SW at the bottom of the ravine. East-west connectivity across the ravine is only provided at the SW Admiral Way corridor.

Replacing the bridges would have significant impacts on traffic mobility during construction. SW Admiral Way would be completely closed or reduced to one or two travel lanes with intermittent full closures. People walking and biking would need to detour to the south on residential streets and cross at the end of the ravine on SW Forest Street, approximately a quarter mile south of SW Admiral Way. Fairmount Ave SW that traverses the ravine under the bridges would need to be completely closed during most of the construction period to allow for construction access and staging.

Because SW Admiral Way is a vital link between the North Admiral neighborhood and the West Seattle Bridge and downtown Seattle, potential detours would require accessing California Avenue SW from either Harbor Avenue SW to the north or SW Oregon Street to the south, as shown in Figure 2.13. In addition, a temporary rerouting of King County Metro Routes 56 and 57 and relocated bus stops would be needed.

Access to several residential houses and private driveways on Fairmount Ave SW north of the bridges would need to be maintained throughout the duration of construction. Access to these residences would only be possible via Harbor Avenue SW while the road is closed for construction. Closing this street to traffic would require a detour to other north-south streets connecting the North Admiral neighborhood and Alki, such as California Avenue SW. Vehicles south of SW Admiral Way traveling north on Fairmount Ave SW would detour at SW Stevens St or SW Forest St. Figure 2.14 shows a proposed detour route for a Fairmount Ave SW closure.

Traffic impacts were evaluated for the 2024 seismic retrofit project. A traffic study identified the need for lane closures and occasional full bridge closures for retrofit construction. A copy of the traffic study is included in Appendix B.

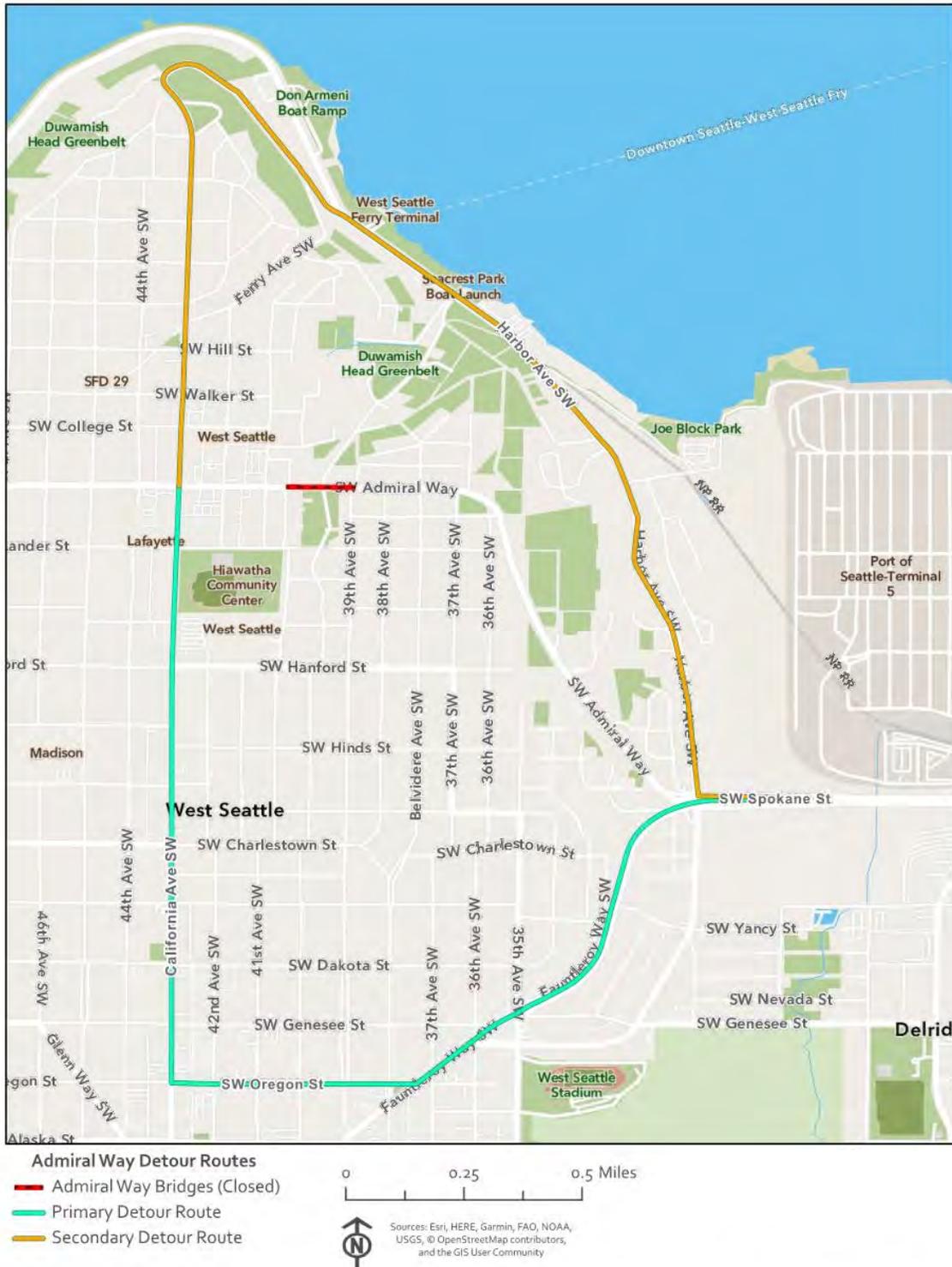


Figure 2.13 SW Admiral Way Detour Routes

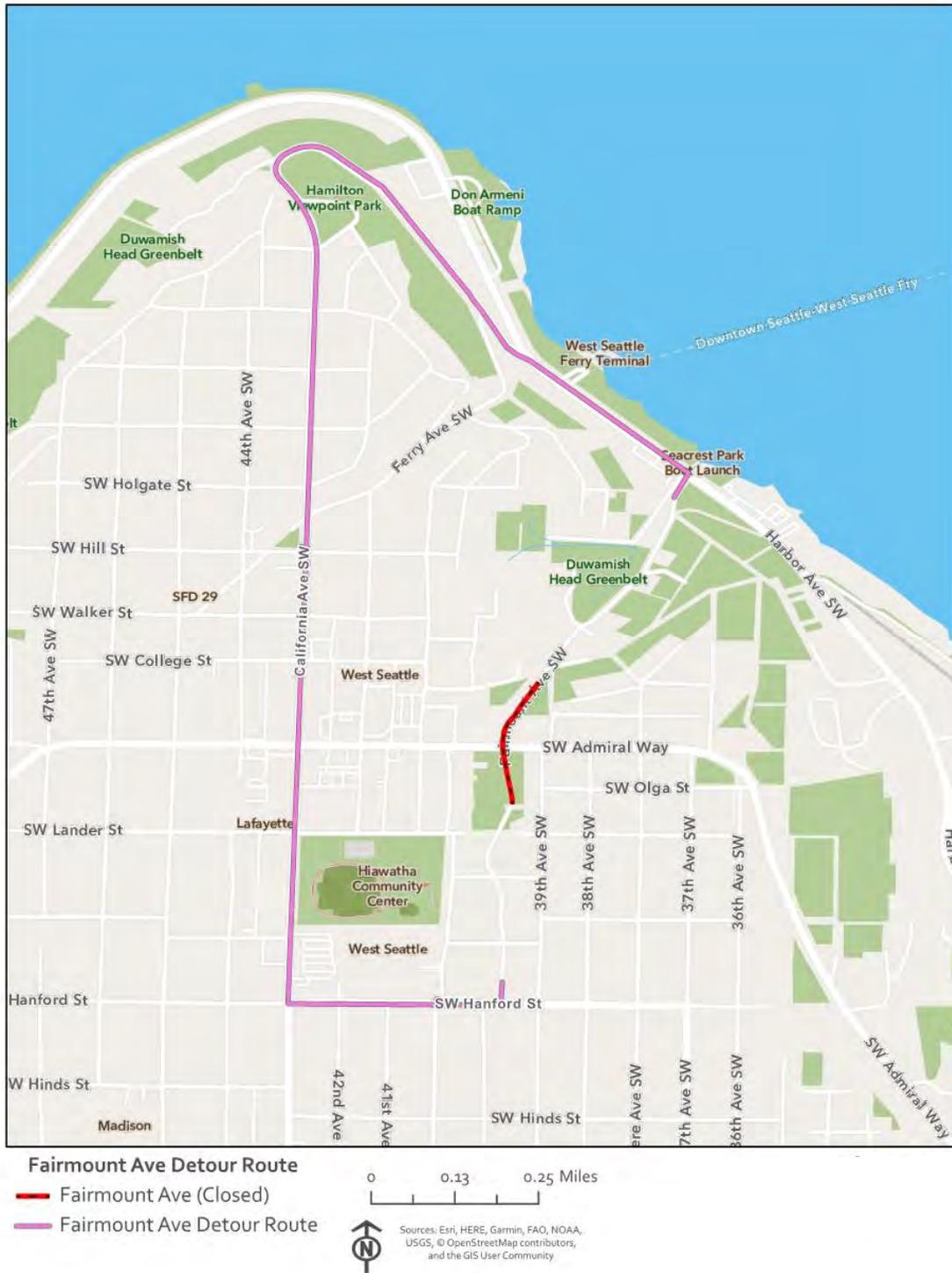


Figure 2.14 Fairmount Avenue SW Detour Route

2.4 Stormwater Management

Stormwater drainage for the Admiral Way Bridges is conveyed to downspouts on the bridge support columns, where it is either piped underground to an SPU-owned stormwater drainage main or discharged to sheet flow into the ditch along Fairmount Ave SW. The bridge runoff is eventually conveyed to an outfall into Puget Sound. It does not appear that the flows are routed to a treatment facility before discharge into Puget Sound. It is expected that the existing drainage patterns would be maintained for a replacement structure.

If there is a preference to eliminate drainage scuppers on the replacement bridge, an assessment of gutter spread would be required to determine if inlet structures are required within the bridge limits. The proposed bridge cross-sections discussed in Section 2.2 include alternatives having a footprint matching the existing width as well as cross sections with expanded widths. The alternatives having an expanded width would increase the pollution-generating area of hard surfaces.

Because the bridges discharge stormwater to a designated receiving water body, flow control is not required. Flow control requirements are summarized in Appendix C in the Minimum Requirements Flowchart of the City's Stormwater Manual. Downstream analysis is required to ensure sufficient capacity in the downstream stormwater drainage system.

Stormwater from the bridges does not discharge to a public combined sewer and thus water quality will need to be evaluated. The existing land cover within the project site boundary is greater than 35% hard surface and more than 5,000 square feet of new pollution-generating hard surface is possible for the expanded width alternatives. Therefore, basic stormwater treatment is required for this project. The target area for water quality treatment will be new pollution-generating hard surfaces.

In addition, the project discharges to a designated receiving water body, which means oil control and enhanced treatment are not required. See the Minimum Requirements Flowchart for water quality treatment requirements. It is expected that 6PPDQ (6 phenyl phenylenediamine-quinone) treatment associated with vehicle tires may be a future environmental requirement in the City of Seattle Stormwater Code or from environmental regulatory agencies.

The best practices for removal of 6PPDQ for different water quality treatment options are currently unclear, but studies are ongoing. It is unknown which treatment options would be best to remove 6PPDQ and some modification of treatment may be required. In-line water quality treatment devices could be a possible water quality solution, and further study is needed to verify that in-line treatment structures are compatible with the bridge structure.

The roadway portion of the project includes more than 2,000 square feet of new plus replaced hard surfaces; therefore, on-site stormwater management will need to be evaluated. However, due to site constraints, such as steep slopes, it is anticipated that on-site stormwater facilities will be infeasible.

2.4 Utilities

The existing bridges carry a limited number of utilities across the ravine. The main utility that crosses the bridge is an active gas line, located along the south edge of the bridges under the sidewalk. This gas main will need to be relocated temporarily or permanently for the bridge replacement that could result in additional traffic impacts.

The bridges also have street lighting on both sides of the roadway, but do not carry electrical power across the ravine. As shown on the Seattle City Light lines map in Figure 2.15, the main electrical crossing is south of the bridge on SW Lander St.

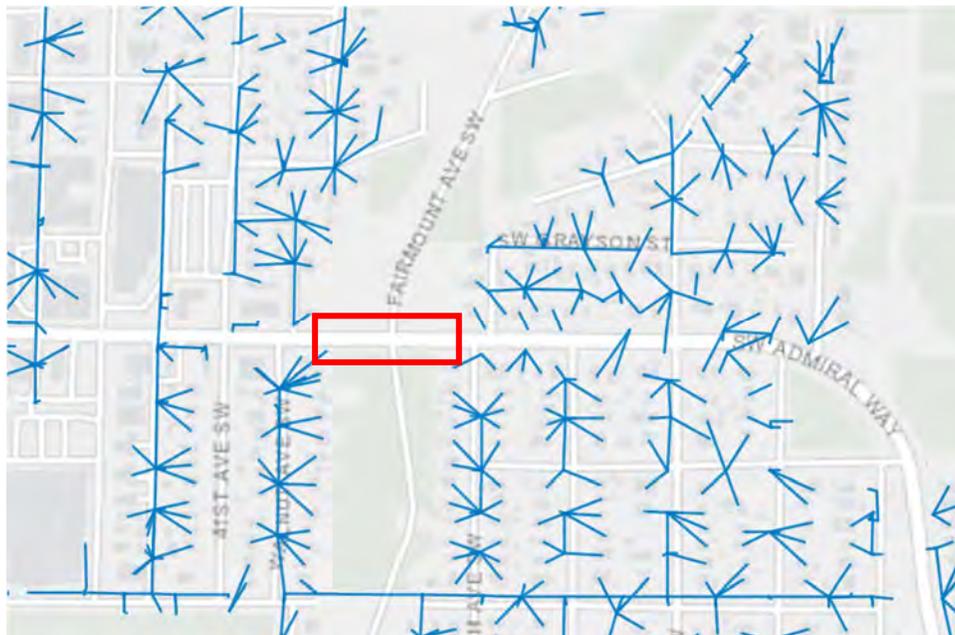


Figure 2.15 Seattle City Light Lines Map

2.5 Geotechnical Conditions

2.5.1 Subsurface Conditions

Two subsurface investigations have been performed in the vicinity of the site over the past 30 years. In 1995, as part of the mid-1990's seismic retrofit project, several shallow test borings were sampled at the bridge abutments and piers and one deep boring was drilled on Fairmount Ave SW underneath the bridge. In 2004, a geotechnical investigation was performed for the condominium building located southwest of the bridge that consisted of test pits, a hand auger boring, and a deep hollow stem boring.

Based on the subsurface explorations, the soils on the slopes under the bridge appear to mainly consist of loose colluvial sand over dense to very dense Advance Outwash Deposits. The loose surface layer is expected to be 10 to 20 feet in thickness, which is consistent with the 11- to 22-foot depth of the existing bridge foundations. However, there are no geotechnical explorations to confirm this thickness, particularly

up and downslope of the bridge foundations. At the bridge approaches, a medium dense, weathered Advance Outwash Deposit is assumed to overlie the very dense Advance Outwash unit.

Given these subsurface conditions, foundations for a new bridge structure will need to extend into the stronger, denser soil layers to provide sufficient vertical and lateral support for the bridge.

2.5.2 Slope Stability

The bridges cross over a steep ravine extending approximately 100 feet below the roadway. The ravine is sloped on average 35 degrees (1.4H:1V) on the west and east sides of Fairmount Ave SW. Figure 2.16 shows an elevation view of the bridges.

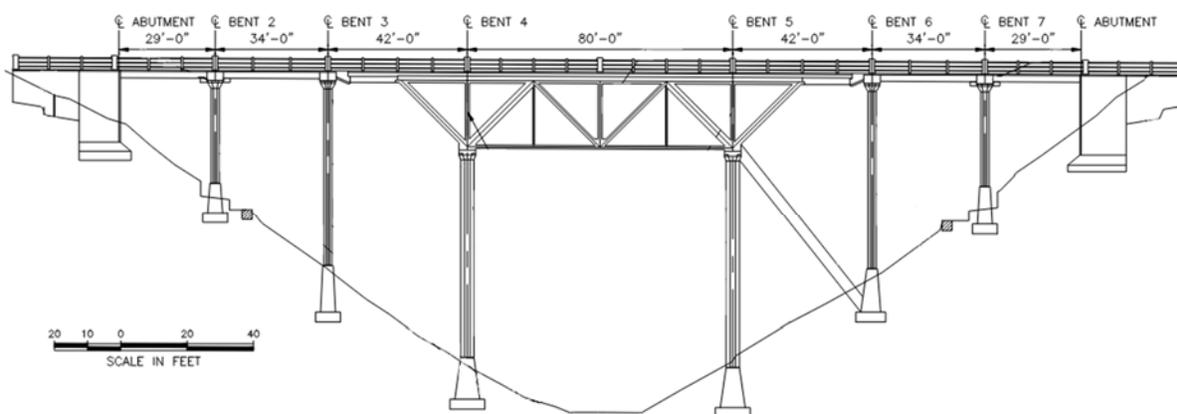


Figure 2.16: Elevation View of North Bridge

The combination of steep slopes and subsurface conditions results in slope stability deficiencies. A slope stability analysis performed as part of the 2024 seismic retrofit project found deficient factors of safety for both static and seismic conditions. As a result, as part of the retrofit project, the slopes are being strengthened using a combination of drilled shafts at the abutments and soil nailing on the steep slopes. Future replacement of the bridges will need to account for slope instability within the ravine.

Erosion under the bridges is also an ongoing issue. Outside the footprint of the bridges, the ravine is heavily vegetated with trees and undergrowth, providing stability to surficial soils on the hillside. Under the bridges, however, there is limited vegetation and the sandy surficial soils are prone to sliding and erosion.

2.5.3 Recommended Future Geotechnical Explorations

Given the limited existing geotechnical data, the design of a replacement bridge will require detailed subsurface explorations to better understand the subsurface soil conditions, particularly the depth and characteristics of the loose surface layer and the underlying Advance Outwash deposits, groundwater conditions, and the dynamic properties of the subsurface soils. A better understating of the subsurface stratigraphy and soil properties will enable development of representative subsurface conditions to be used for foundation design and seismic hazards.

3. Alternatives Considered

Several alternatives were considered for the rehabilitation/replacement of the Admiral Way Bridges and evaluated for further consideration. These included the following:

- Bridge replacement constructed in a single stage with SW Admiral Way completely closed during construction
- Bridge replacement constructed in two stages allowing limited traffic on SW Admiral Way during construction
- Bridge rehabilitation (no replacement)

In addition to these alternatives, other options were considered as summarized below. They were found to be undesirable and were not considered further.

- A phased bridge implementation that would replace the bridges one at a time over several years. This approach would provide funding flexibility if there were insufficient funds to replace both structures at the same time. However, this approach has significant disadvantages including cost inefficiencies and environmental and traffic impacts of two separate construction projects.
- Replacing only the North bridge and leaving the South bridge in place. Given that the North bridge is in worse condition, it would be replaced and the South bridge would remain in service. This approach also has disadvantages that make it undesirable. The remaining South bridge would need to be laterally supported by the new bridge since it would not be able to withstand a design-level earthquake as a standalone structure. In addition, given the advanced age of the South bridge, the cost to maintain it is expected to increase significantly over time, making maintenance and repair impractical and resulting in an eventual need to replace the bridge.

Due to the density of residential neighborhoods south of SW Admiral Way and lack of arterial connections from east of the ravine to California Ave SW, there are no feasible nearby alternatives to a major east-west crossing outside of the SW Admiral Way corridor. Therefore, bridge replacement options were confined to the existing right-of-way corridor on SW Admiral Way.

For the purposes of this study, the two bridge replacement options were similar in all respects except for the construction technique, which mainly affects maintenance of traffic during construction, the need for detours, public safety, and construction duration and cost. In addition, the number of columns/shafts supporting the bridge is different between the two options, with the two-stage construction requiring an extra column/shaft at each bent to accommodate construction of each half of the bridge separately.

Given the width of the ravine, a replacement bridge would be approximately 300 feet in total length, matching the length of the existing bridges, and is anticipated to consist of three spans of approximately the same length to cross over Fairmount Ave SW. An elevation view of the bridge is shown below in Figure 3.1. The span arrangement and structure type are preliminary and will be evaluated in a subsequent phase of project development as part of a Type, Size, and Location (TS&L) study.

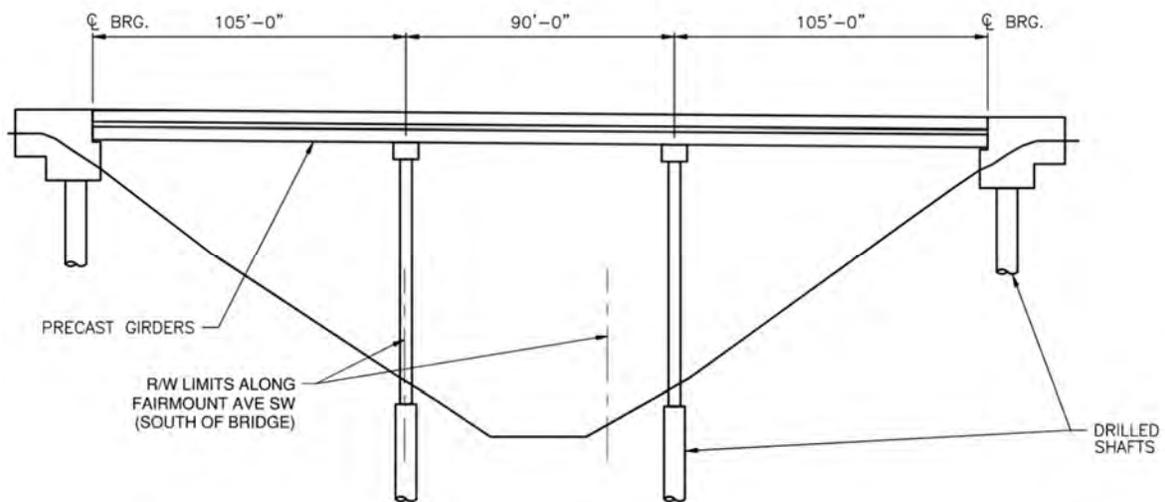


Figure 3.1 Elevation View of Proposed Replacement Bridge

As part of the planning process in considering mobility improvements for all modes of travel, an east-west crossing over the Fairmount Ave SW ravine for people walking and biking was considered to supplement the replacement of the Admiral Way Bridges. However, as shown below in Figure 3.2, the existing at-grade crossing at SW Forest St to access SW Lander St provides a connection between California Ave SW and SW Admiral Way at signalized intersections. This alternate route for people who walk and bike provides another east-west crossing opportunity to divert bicycle traffic from the busy SW Admiral Way corridor. This would also be a good detour route for people biking when bridge replacement is under construction.

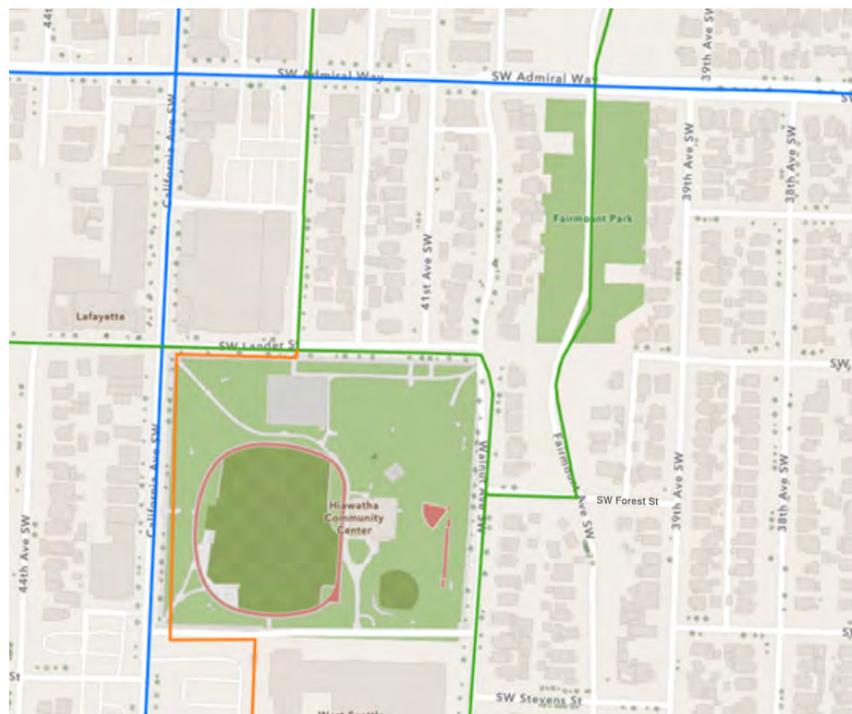


Figure 3.2 Bicycle/Pedestrian Routes Near SW Admiral Way (Source: 2024 Seattle Transportation Plan)

3.1 Alternative 1 – Bridge Replacement with Single Stage Construction

In this alternative, SW Admiral Way would be completely closed to traffic for the duration of construction to allow for the removal and replacement of the existing bridges. Fairmount Ave SW below the bridge would also be closed to allow space and staging areas for construction. This alternative is shown in Figure 3.3 assuming a prestressed concrete girder bridge with drilled shaft foundations and a bridge width matching the existing bridges. A preliminary layout of this alternative is shown in Appendix G, Figure 3.

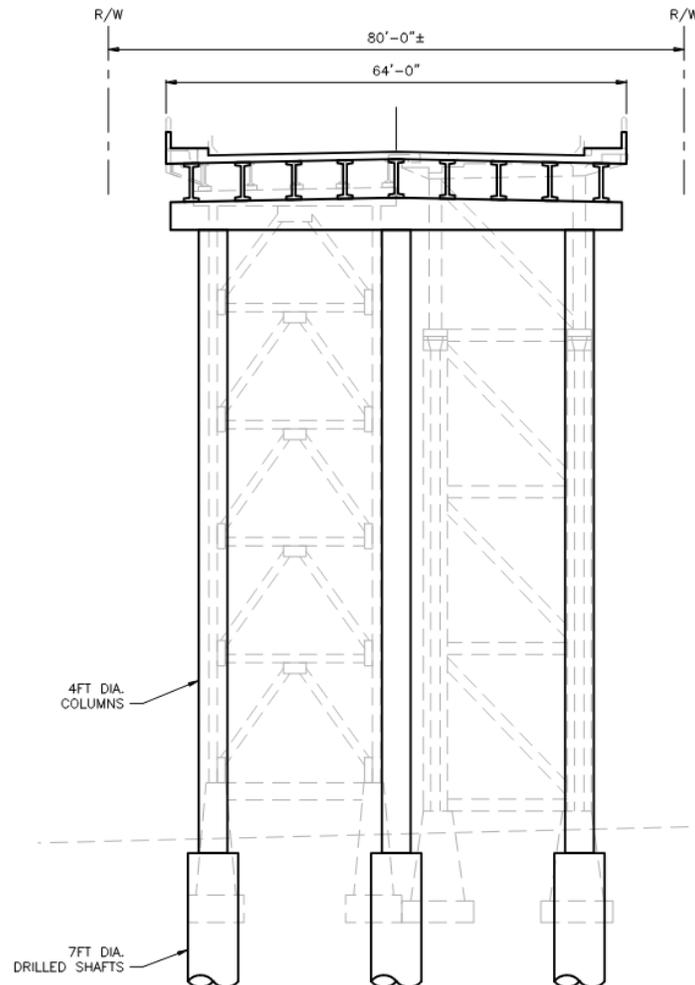


Figure 3.3 Alternative 1 – Single Stage Construction

This alternative provides full access to the construction site and allows for different approaches to remove the existing bridges and construct the new structure. For example, the existing bridges could be removed all at once to provide space for a temporary work trestle or the bridges could be removed in increments, allowing the contractor to use the existing bridges to facilitate construction.

A single stage construction approach provides for a safe work zone separated from the traveling public and allows the most flexibility in how the structures would be replaced. This approach is expected to result in a more cost-effective solution with a shorter duration of construction.

3.2 Alternative 2 – Bridge Replacement with Staged Construction

This alternative consists of a phased approach to replacing the bridges, allowing SW Admiral Way to remain open to traffic during construction. In the first stage, one of the existing bridges would be demolished and reconstructed while maintaining traffic flow on the remaining bridge. Upon completion of the first stage, traffic would be diverted to the newly constructed bridge, allowing for the replacement of the second half of the bridge. The two structures would then be joined at the deck and piers to function as a single bridge.

This alternative is shown below in Figure 3.4 with the North bridge being replaced while traffic is moved to the South bridge. Construction of each stage of the bridge requires two columns and shafts at each pier, resulting in an additional column and shaft per pier. A preliminary layout of this alternative is shown in Appendix G, Figure 4.

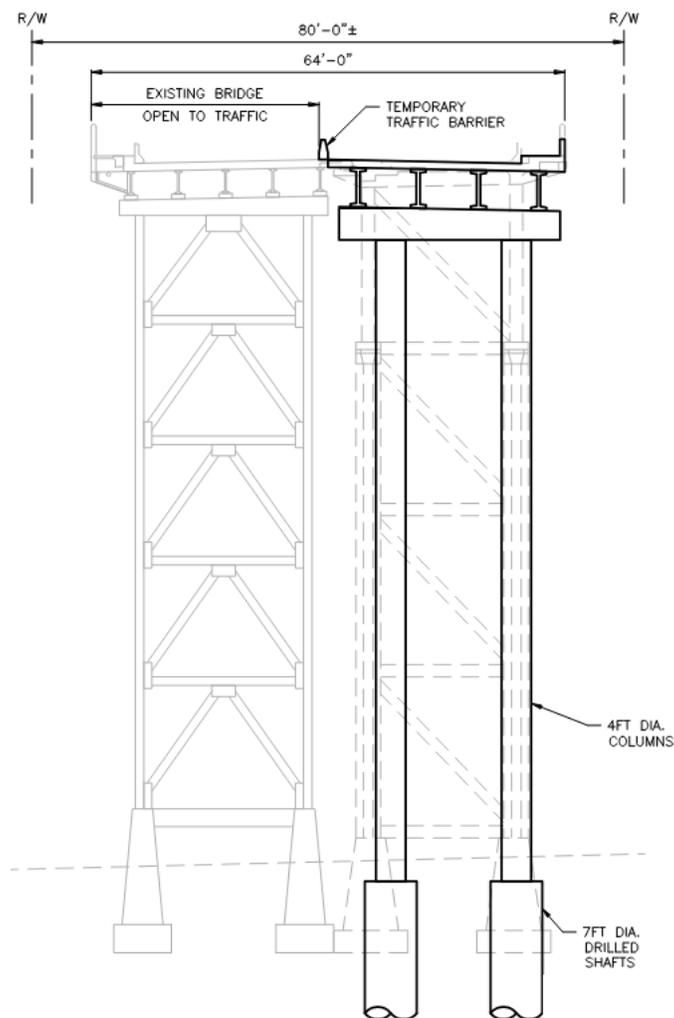


Figure 3.4 Alternative 2 – Staged Bridge Construction (First Stage Shown)

This staged approach to replacing bridges is a commonly used construction practice and allows for some level of traffic to be maintained during construction. It reduces disruption to the local community and maintains connectivity, albeit with limitations due to the reduced number of traffic lanes and the need for occasional closures during critical construction phases. However, replacing the bridges in stages poses more safety risks and constrains the space available for construction. It is expected that the construction period would be significantly longer than that of single stage construction.

3.3 Alternative 3 – Bridge Rehabilitation

An alternative to replacing the bridges is to rehabilitate them to extend their service life. The bridges were built at different times, with the older concrete North bridge constructed in 1927 and the newer steel girder bridge built in 1949. The North bridge has far exceeded a typical 75-year service life and both bridges are showing clear signs of deterioration.

As part of the 2024 seismic retrofit project, limited field condition surveys were conducted. Figure 3.5 shows some locations of cracking, spalling, and corrosion on the North bridge. The South bridge, consisting of painted steel plate girders and steel frame pier bents, is in better condition, with some limited areas of corrosion. As part of the current retrofit project, these areas of deterioration are planned to be repaired.

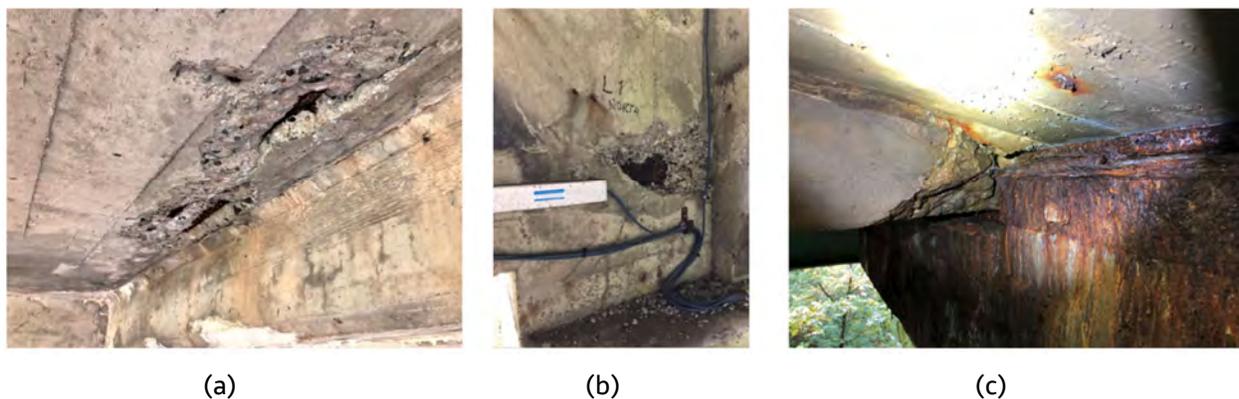


Figure 3.5 Locations of Deterioration of North Bridge

(a) Cracking and spalling of concrete at underside of bridge deck

(b) Spalling of concrete at concrete truss joint

(c) Corrosion of expansion joint bearing seat at end of drop-in span

Given the condition of the bridges and SDOT's long-term goal to replace them, the 2024 seismic retrofit and rehabilitation efforts focused on extending the life of the bridges by at least 20 years. Based on the FHWA Seismic Retrofitting Manual, the seismic retrofit project places the bridges under anticipated service life category ASL2 (16 to 50 years of anticipated service life), as shown in Table 3.1.

Table 1-1. Service life categories.

SERVICE LIFE CATEGORY	ANTICIPATED SERVICE LIFE	AGE (if not rehabilitated) ¹
ASL 1	0 - 15 yrs	60 - 75 yrs
ASL 2	16 - 50 yrs	25 - 60 yrs
ASL 3	> 50 yrs	< 25 yrs
Note: 1. Age is calculated assuming total service life is 75 years and the bridge has not been rehabilitated in its lifetime to date.		

Table 3.1 FHWA-Recommended Service Life Categories (Source: FHWA Seismic Retrofitting Manual)

This service life category leads to a recommended Performance Level of PL1 under a 1,000-year return period seismic event, as shown in Table 3.2.

Table 1-2. Minimum performance levels for retrofitted bridges.

EARTHQUAKE GROUND MOTION	BRIDGE IMPORTANCE and SERVICE LIFE CATEGORY					
	Standard			Essential		
	ASL 1	ASL 2	ASL 3	ASL 1	ASL 2	ASL 3
Lower Level Ground Motion 50 percent probability of exceedance in 75 years; return period is about 100 years.	PL0 ⁴	PL3	PL3	PL0 ⁴	PL3	PL3
Upper Level Ground Motion 7 percent probability of exceedance in 75 years; return period is about 1,000 years.	PL0 ⁴	PL1	PL1	PL0 ⁴	PL1	PL2

Table 3.2 FHWA-Recommended Performance Levels (Source: FHWA Seismic Retrofitting Manual)

A performance level of PL1 denotes that a minimum requirement of life safety is maintained during an earthquake, although the bridge may be heavily damaged and in need of replacement after an earthquake. There is no requirement to account for vehicle loading or other kinds of day-to-day service level considerations under the FHWA seismic retrofitting methodology. Therefore, an assessment of service level performance needs to be obtained by other means.

The bridge deck and superstructure were recently evaluated through load rating of the bridges. Based on a 2018 Load Rating prepared by Bridge Diagnostics Incorporated (BDI), the South bridge was found to have a controlling LRFR load rating factor of 1.17 for Inventory HL-93 loading. The North bridge was found to have a lower controlling load rating factor of 0.82 for this same load, producing positive moment at the end approach slabs. In general, the North bridge appears to be controlled by the capacity of the bridge deck. The report concludes that the structures can safely support both legal and SDOT permit loads.

The steel-reinforced concrete truss is another critical load-carrying element of the North bridge. As part of the 2024 seismic retrofit design, the effect of closing the expansion joints in the bridge deck at the ends of the truss was assessed. The truss was modeled and analyzed for vehicle loading and it was found that the truss had relatively low, but acceptable, capacity to demand ratios. The minimum capacity to demand ratio was 1.08 for HL93 loading. The critical elements of the truss are highlighted in red in Figure 3.6 below.

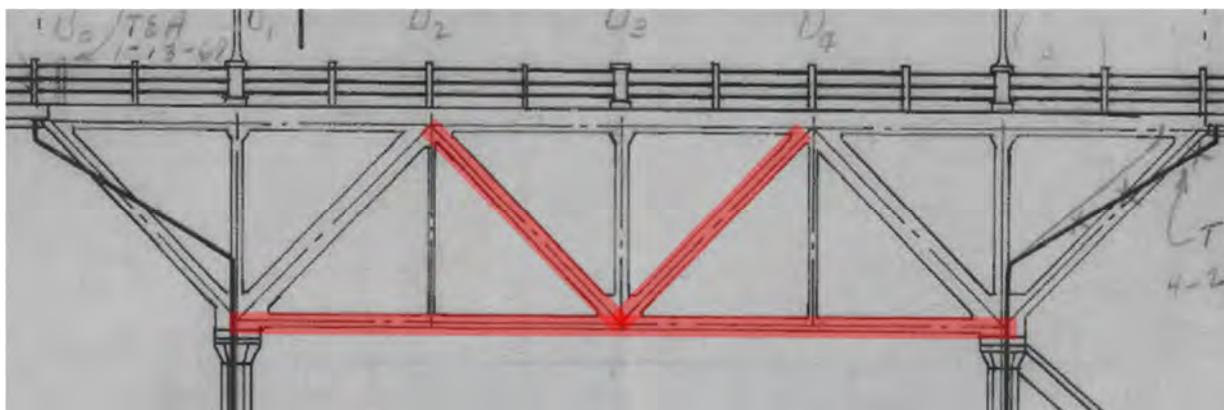


Figure 3.6 Critical Elements of Truss Under Vehicle Loading at North Bridge

Given this information and recent field observations of the bridges, it is expected that, over time, the North bridge will become increasingly more costly to maintain in operation. The deck slabs and concrete truss already show signs of deterioration and will need to continue to be repaired to extend the service life of the bridge. Rehabilitating these elements would be costly and have significant impacts on traffic.

There is also concern about the condition of the concrete corbels at the ends of the truss that support the suspended deck slabs, as they show signs of substantial corrosion (see Figure 3.7). These components are critical life-safety elements and the continued maintenance of them is essential to maintaining bridge operations.



Figure 3.7 Concrete Corbel at End of Concrete Truss (North Bridge, Looking East)

Bridge rehabilitation offers the lowest short-term costs, the least impacts to the traveling public, a streamlined permitting process, and fewer adverse effects to the NRHP-eligible bridge. However, rehabilitation would not eliminate the need for continuing maintenance investments compared to a new structure, rendering it an unfavorable option. Therefore, any further analysis in this document will focus on alternatives that involve replacement of the bridges.

4. Alternatives Analysis Methodology and Evaluation

This section describes the approach and methodology used to evaluate bridge replacement alternatives. A performance-based approach was employed using the project purpose and goals as a basis for establishing evaluation criteria to compare the alternatives.

The evaluation criteria fall into the following five categories, as shown in Figure 4.1:

- Cost Considerations
- Implementation Characteristics
- Mobility and Connectivity
- Environmental and Permitting Considerations
- Stakeholder Input

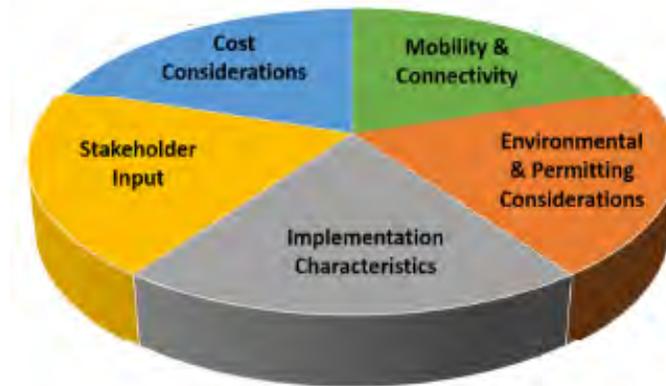


Figure 4.1 Evaluation Categories

Within each category, project-specific criteria were identified to evaluate the alternatives. Table 4.1 summarizes the evaluation categories and criteria. The alternatives were compared using a screening matrix with each category given a weighting. By varying the relative weighting of categories, it was possible to assess the sensitivity of the scoring to a particular evaluation category.

The screening process was conducted by the project team that included representation from the following departments within SDOT:

- Project Development
- Roadway Structures
- Transportation Operations
- Construction Management
- Structural Engineering Services
- Environmental Services
- Real Property
- Communications and Public Engagement

Cost Considerations	Implementation Characteristics	Mobility & Connectivity	Environmental & Permitting Considerations	Stakeholder Input
25% weight	20% weight	20% weight	20% weight	15% weight
Design and construction	Construction duration	Vehicle, freight, and transit access and connectivity	Impacts to adjacent land use and Environmentally Critical Areas (ECA)	Noise and vibration impacts
Inspection and maintenance	Constructability	Bike and pedestrian access and connectivity	Visual impacts	Temporary and permanent utility relocation
Permanent easement requirements	Public Safety	Need for detour routes	GSI and stormwater runoff	Public and agency input
	Construction easement requirements	Other considerations	Property acquisition	
			Historic preservation	

Table 4.1 Evaluation Criteria with Weighting of Categories

4.1 Cost Considerations

Cost considerations comprise both initial capital cost of bridge replacement and life cycle costs that include inspection, maintenance, and possible rehabilitation over the service life. Recognizing the importance of allocating funding for a bridge replacement project, a weighting factor of 25% was used for this category.

Class 4 cost estimates were prepared for the bridge replacement alternatives in accordance with the Association for the Advancement of Cost Engineering (AACE). Class 4 estimates are appropriate for a 1 to 15% level of project definition and typically have an accuracy range of -15% to -30% on the low end and +20% to +50% on the high end, depending on the complexity of the project and the level of contingency used.

Using an accuracy range of -20% to +50%, the estimated total project cost of the single-stage bridge replacement is \$22.8 to \$42.8 million in 2024 dollars. The staged bridge replacement approach, on the other hand, has an estimated cost of \$29.8 to \$55.8 million, approximately 30% higher than the single-

stage alternative. A breakdown of the conceptual cost estimate is provided in Appendix D. The economy of a single-stage construction method results from providing full access to the construction site with the ability to develop creative and cost-effective approaches to replace the bridges. A staged replacement that requires traffic to operate within the construction site significantly lessens the efficiency of the work in the relatively narrow work zone and extends the construction timeline by several months.

Both alternatives are expected to have similar life cycle costs since the structures themselves would be essentially identical. The two-stage bridge, with an extra column and shaft foundation at each bent, might carry higher inspection and maintenance costs over the service life of the bridge.

4.2 Implementation Characteristics

Implementation characteristics relate to the manner in which the project is implemented, specifically how the bridges are replaced. There are several criteria to consider, including constructability, construction duration, public safety, and the need for temporary construction easements.

The single stage bridge replacement alternative, in which SW Admiral Way is closed for the duration of construction, is noted for its shorter construction duration and improved constructability. For the single stage method of construction, access to the full width of the roadway allows flexibility in how the bridge is constructed. Erecting the girders, for example, can be accomplished in a more efficient manner. A possible girder erection sequence for the two alternatives is shown in Appendix G, Figures 5 and 6. A constructability evaluation was performed for the two replacement alternatives and is included in Appendix E.

It is also expected that a single stage approach would be constructed several months faster than a two-stage approach in which construction must accommodate traffic through the work zone. A shorter construction period would be a benefit to the traveling public and the surrounding community by shortening the inconvenience of road construction. Conceptual-level construction schedules for the two alternatives are included in Appendix F.

4.3 Mobility and Connectivity

Mobility and connectivity also differentiate the two alternatives. The single-stage construction approach does not allow traffic flow on SW Admiral Way during construction, necessitating full detours for vehicles, pedestrians, and people biking. On the other hand, closing SW Admiral Way for the duration of construction, although inconvenient, would result in the use of consistent detour routes that increases predictability for the general public, freight, and transit.

By comparison, replacing the bridges in two stages allows vehicles to use the SW Admiral Way corridor during construction. Two lanes of traffic (one in each direction) could be maintained for the majority of time, although there would be times during the workday that construction operations would require temporary traffic closures. The uncertainty of knowing when the bridge is open to traffic could lead to congestion on SW Admiral Way and motorists cutting through the neighborhood south of the bridge to avoid traffic backups.

4.4 Environmental and Permitting Considerations

Environmental and permitting considerations are an important evaluation category given the environmentally sensitive ravine that the bridges span across. Demolition of the existing structures and construction of a new bridge will have temporary impacts in the ravine that will need to be mitigated for. Both bridge replacement alternatives are similar in the expected environmental impacts and permitting requirements associated with construction, although the staged construction approach would require a longer period of construction and associated impacts within the ravine.

From a permitting standpoint, replacement of the bridges will remove the North bridge, which is a National Register of Historic Places (NRHP) eligible bridge. This bridge was built adjacent to an original wooden trestle in 1927 to ease congestion along SW Admiral Way and features a main span consisting of an 80-foot long concrete Warren deck truss. The bridge is recommended as eligible for listing in the NRHP under Criterion A for its association with the early twentieth century development and growth of West Seattle. It is also recommended as eligible under Criterion C as a good example of a concrete Warren deck truss bridge.

In addition to NRHP criteria, the North bridge may meet Seattle Landmarks Criterion F because of its prominence of spatial location, contrasts of siting, age, or scale, it is an easily identifiable visual feature of its neighborhood and contributes to the distinctive quality or identity of the neighborhood or city. The Seattle Landmarks Preservation Board determines whether a property meets this threshold.

Removal of the NRHP-eligible North bridge would not meet the exemption criteria of SEPA (State Environmental Policy Act), requiring permitting under NEPA (National Environmental Policy Act).

4.5 Stakeholder Input

Community impacts are an important consideration in evaluating bridge replacement alternatives. Below are the key stakeholders for this project:

- West Seattle community
- Public and nearby residents
- Local business community
- King County Metro
- Seattle Public Utilities (SPU) and Seattle City Light (SCL)
- Seattle Parks and Recreation
- Utility Providers
- Cascade Bicycle Club
- West Seattle Bike Connections

Outreach Survey

In addition to the technical work done to assess the options for bridge replacement, SDOT engaged with the local community to better understand their preferences for the SW Admiral Way corridor. Outreach was conducted primarily through an online public survey in January-February 2024 that was promoted

with advertisements placed on social media and local news outlets. The social media ads were geofenced (shown to people in a specified geographic region) to community members in the area of the Admiral Way Bridges and the Admiral Urban Village. The survey was translated into Spanish, Chinese, Vietnamese, Somali, Korean, and Amharic with social media ads translated into their respective languages.

The main goal of the survey was to understand how residents, businesses, and general travelers use the Admiral Way Bridges, their preferences for construction timeline, and how they would like the replacement bridge to serve users in the future.

The survey asked the following questions:

- Why do you use the Admiral Way Bridges and how do you use them?
- How often do you travel over or under (along Fairmount Ave SW) the Admiral Way Bridges?
- Whenever we do replace the Admiral Way Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities?
- How about the construction timeline – which would you prefer? A longer construction period if a part of the bridge can still be used or quicker construction even if it means no bridge access?
- Construction will bring some changes. What are your top three concerns?
- If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?
- We're curious how you found out about this project. Can you let us know?

For a complete copy of the survey questions and results, refer to Appendix I.

Survey Results Summary

The results of the survey showed that people using the Admiral Way Bridges are heading to a variety of destinations and using the bridges to go to “fun activities” receiving the most responses, followed by “running errands,” “visiting friends and family,” and “going to work or school.” Respondents overwhelmingly identified driving as their primary mode across the bridges and to their destinations, with 80-90% of respondents using a motor vehicle. The bridges are traversed regularly by survey respondents, with 47% reporting using the bridge on a weekly basis and 22% using it daily.

When asked to rank the top 3 priorities a future replacement bridge should plan for and accommodate, respondents overwhelmingly favored prioritizing non-motorized users, with 72% saying that it should be better for people walking, 59% saying it should be better for people biking, and 42% saying it should be easier for people taking transit. The survey responses to this question are shown in Figure 4.2 below.

ANSWER CHOICES	RESPONSES	
It should be easier for people biking.	59.30%	51
It should be better for people walking.	72.09%	62
We should improve it for people driving.	39.53%	34
It should be more convenient for people who use public transport.	41.86%	36
It should accommodate freight better.	11.63%	10
Keeping the bridge's look or its historical features is important.	37.21%	32
Something else (please tell us more).	5.81%	5
Total Respondents: 86		

Figure 4.2 Survey Responses – Top Priorities for a Replacement Bridge

Regarding temporary construction impacts, respondents were most concerned with prioritizing detour routes for non-motorized and motorized users alike with 62% of respondents saying that transit routes should remain open and accessible during construction.

One important focus of this planning study is to assess the construction methods and timeline for replacement of the bridges. The survey asked respondents whether they would prefer a longer construction period if they could still use a portion of the bridge or a shorter construction period with the bridge completely closed. Almost 70% of survey respondents preferred a longer construction timeline with reduced access to the bridge, as shown in Figure 4.3 below.

Q4 How about the construction timeline – which would you prefer?

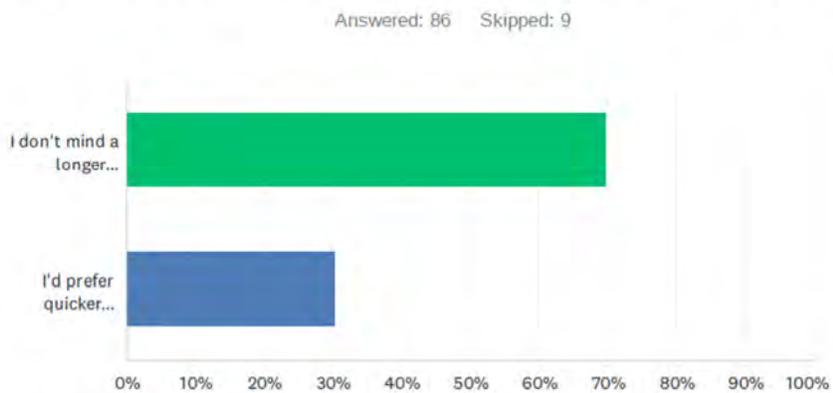


Figure 4.3 Survey Responses – Preferred Construction Timeline

4.6 Assessment Results and Sensitivity Analysis

After assessing how the two bridge replacement alternatives met the evaluation criteria, they were scored in the screening matrix shown below in Table 4.2. The relative weighting of the evaluation categories was based on professional judgement and input from SDOT.

CRITERIA	SUGGESTED WEIGHT	ALTERNATIVES	
		1 - Bridge replacement in a single stage	2 - Two stage bridge replacement
Cost considerations	25%	10	8
Design and construction		Base design	More costly to construct
Inspection and maintenance		Relatively low maintenance costs	Relatively low maintenance costs
Permanent Easement Requirements		Lowest chance of permanent easements	Low chance of permanent easements
Implementation Characteristics	20%	10	5
Construction duration		Shorter duration of construction	Longer duration of construction
Constructability		More latitude for contractor's means and methods	Narrow work zones for entirety of construction, requires more traffic control
Public Safety		No conflict or interaction between construction work and public access	Significant conflict and interaction between construction work and public access
Temporary Construction Easement (TCE) requirements		~30 ft wide easement on north and south sides of bridge	~30 ft wide easements, likely additional TCEs to facilitate construction
Mobility & Connectivity	20%	5	10
Vehicle, transit, and freight access & connectivity		Admiral Way closed during construction, requiring detours	Two lanes of traffic (one in each direction) can be maintained at most times
Bike and pedestrian access & connectivity		Admiral Way closed during construction, requiring detour to SW Forest St	One sidewalk maintained at most times
Need for detour routes		Need to detour all traffic and pedestrians during construction	Intermittent detours needed
Other considerations		Fairmount Ave SW would be closed for approx. 12 months during construction	Fairmount Ave SW would be closed for approx. 18 months during construction
Environmental & Permitting Considerations	20%	10	10
Impacts to adjacent land use		No impacts expected unless replacement bridge is wider	No impacts expected unless replacement bridge is wider
Impacts to Environmentally Critical Area (ECA)		Construction impacts to vegetation in ravine to be restored	Construction impacts to vegetation in ravine to be restored
Visual impacts		No impacts expected	No impacts expected
GSI & stormwater runoff impacts		Opportunity to provide stormwater treatment with new bridge	Opportunity to provide stormwater treatment with new bridge
Property acquisition		TCEs reqd in ravine on each side of bridge	Additional TCEs likely needed at corners of bridge
Historic preservation		Eligible for listing in Nat'l Register of Historic Places	Eligible for listing in Nat'l Register of Historic Places
Community Impacts	15%	10	8
Noise and Vibration Impacts		Shorter overall duration of impacts and only 1 mobilization of shaft equipment	Longer duration of impacts and 2 mobilizations of shaft equipment
Utility Relocation		Minor impacts - existing gas line to be temporarily relocated	Minor impacts - existing gas line to be relocated one or two times during construction
Public and agency input		From outreach survey, community prefers longer construction timeline if full closure of SW Admiral Way avoided	From outreach survey, community prefers longer construction timeline if full closure of SW Admiral Way avoided
WEIGHTED SCORE (x 100)		900	820
COMPARATIVE PROJECT COST (\$MIL)		\$28.5 M	\$37.2 M
BENEFIT/COST SCORE		31.6	22.0
RANK		1	2

NOTES:

1. For each criteria, alternatives are ranked between 1-10, with 10 being most favorable and the other alternative scored relative to that one.
2. Comparative project costs are Rough Order of Magnitude (ROM) costs. See Appendix D for conceptual cost estimates.

Table 4.2 Alternatives Screening Matrix

As Table 4.2 shows, Alternative 1 has a higher weighted score and lower cost. A sensitivity analysis was performed to assess how the comparison of alternatives changed as relative weighting of the evaluation criteria was varied. As can be seen in Table 4.3 below, Alternative 1 had the highest weighted score for all relative weights considered.

Criteria	Weight	1 - Bridge replacement in a single stage	2 - Two stage bridge replacement
1. Base Case			
Cost considerations	25%	10	8
Implementation Characteristics	20%	10	5
Mobility & Connectivity	20%	5	10
Environmental & Permitting	20%	10	10
Community Impacts	15%	10	8
BENEFIT SCORE (x 100)		900	820
COMPARATIVE PROJECT COST (\$MIL)		\$28.5 M	\$37.2 M
BENEFIT/COST SCORE		31.6	22.0
RANK		1	2
2. Case A (focusing on Connectivity/Environmental/Community Impacts)			
Cost considerations ↓	15%	10	8
Implementation Characteristics ↓	15%	10	5
Mobility & Connectivity ↑	25%	5	10
Environmental & Permitting ↑	25%	10	10
Community Impacts ↑	20%	10	8
BENEFIT SCORE (x 100)		875	855
BENEFIT/COST SCORE		30.7	23.0
RANK		1	2
3. Case B (focusing on Costs / Implementation Characteristics)			
Cost considerations ↑	30%	10	8
Implementation Characteristics ↑	25%	10	5
Mobility & Connectivity	20%	5	10
Environmental & Permitting ↓	15%	10	10
Community Impacts ↓	10%	10	8
BENEFIT SCORE (x 100)		900	795
BENEFIT/COST SCORE		31.6	21.4
RANK		1	2

Table 4.3 Results of Sensitivity Analysis

5. Conclusions

5.1 Key Findings

Below is a summary of the key advantages and disadvantages of each alternative.

Bridge Replacement, Single Stage Construction

- **Advantages:**
 - Lower estimated construction cost
 - Shorter construction duration leading to shorter impacts to the traveling public
 - More opportunities for creative means and methods of construction
 - Minimized traveling public interaction with construction activities.
- **Disadvantages:**
 - Significant disruption to traffic flow requiring extensive detours for motorists
 - Substantial impact on local community and nearby businesses during construction
 - Removal of a National Register of Historic Places (NRHP) eligible bridge

Bridge Replacement, Staged Bridge Construction

- **Advantages:**
 - Maintain partial traffic flow on SW Admiral Way, reducing the need for detours
 - Less direct impact on the local community
 - Allow for gradual adaptation to construction activities
- **Disadvantages:**
 - Higher estimated construction cost
 - Prolonged construction period leading to extended community exposure to construction activities
 - Potential for construction fatigue among local residents and businesses
 - Increased traveling public interaction with construction activities
 - Removal of a National Register of Historic Places (NRHP) eligible bridge

Rehabilitation of Existing Bridges

- **Advantages:**
 - Lowest initial financial outlay
 - Minimal disruption to traffic during rehabilitation work
 - Maintain current traffic patterns and connectivity
 - Lower adverse effect to a National Register of Historic Places (NRHP) eligible bridge
 - More streamlined permitting process due to SEPA exemption

- **Disadvantages:**
 - Potential for higher long-term maintenance costs
 - May not fully address functionally obsolete elements to current design standards
 - Likelihood of recurring disruptions for ongoing maintenance in the future

5.2 Recommendations

Based on the evaluation of alternatives, bridge replacement in a single stage of construction appears to be a cost-beneficial and operationally viable option, with the primary challenge being the impact of closing SW Admiral Way to the traveling public during construction. The staged construction approach, while more costly and prolonged, offers a balance between construction progress and maintaining a degree of traffic flow.

Bridge rehabilitation, despite its lower initial cost, may not fully address all the functionally obsolete elements to current design standards and does not address increasing maintenance costs. Given the advanced age of the bridges beyond their service life, the cost to maintain the bridges is expected to increase significantly over time, eventually making ongoing maintenance impractical. This alternative is not recommended.

Given the complexities and varied impacts of the bridge replacement alternatives, it is recommended that both bridge replacement alternatives be considered as viable approaches when the project is further developed. Continuing engagement with the local community and project stakeholders to assess community preferences and concerns, especially regarding traffic management and community disruption during construction, will be valuable. Further detailed engineering studies and environmental impact assessments should be conducted to refine cost estimates and develop implementation strategies in the next stage of project definition.

Appendix A. Complete Streets Checklist

COMPLETE STREETS CHECKLIST

For projects over \$500,000

Project Name:

Project Developer:

Phone Number:

Opportunity Statement (See Project Roadmap for instructions):

Description of scope from originating project:

Project Extent:

Project Budget and Funding Source(s) (List modal plans if applicable):

If grant funded, list timeline:

1 Purpose of the Complete Streets Checklist

Seattle's Complete Streets policy is about creating and maintaining safe streets for everyone. In 2007, the Seattle City Council passed Ordinance 122386, known as the Complete Streets ordinance, which directs Seattle Department of Transportation (SDOT) to design streets for people who walk, bike, ride transit and people of all ages and abilities while promoting safe operation for all users, including cars and freight. This is the lens through which SDOT views all of our projects.

SDOT uses a rigorous, data-driven process to develop complete streets. Streets Illustrated provides the roadmap of how SDOT administers the Complete Streets program. The Complete Streets checklist is the tool SDOT uses to collect data and information about the status of the street and surroundings, as well as the details of the project during a project's complete streets assessment in the early stages of the 0-30% design phase, with a goal of identifying specific improvements that can be incorporated into the project to balance the needs of all users.

2 Complete Streets Review Story Map - Getting Started

Data pertaining to questions in this checklist can be found in the [Complete Streets Review Story Map](#). To use the map you need to know the following:

Using your mouse wheel or the zoom controls at the top left corner of the map, zoom to your project area. As you will notice, zooming in makes new data appear on the map. Alternatively, you can use the search tool (magnifying glass button) to type in an address for the location you are looking for. For the most accurate results, include both the city (Seattle) and the state (WA) after the address.

Once you have reached an acceptable scale, begin by clicking on step #3 (Arterial Classification and Street Type), and click on the following numbered tabs to see just the relevant data for each topic. Each map will preserve the scale of the previous map.

Within each map you can click on any of the features to get whatever information is associated with that layer. Because these maps include data with the same shapes, you may need to zoom in or out to see other available layers.

Summary:

- Some data layers will only display at a distinct scale, you will need to zoom in or out on the maps to find and view all the layers you need for each topic.

Questions or comments about the checklist template?

Please email Gabriel Seo (gabriel.seo@seattle.gov) for more information.

3 Project Coordination

1. Review [DOTMaps](#), the [Complete Streets Review Story Map](#) and associated links. Are there any opportunities to coordinate with relevant City projects/initiatives within the project area? Yes No

Describe final decision:

Discuss coordination opportunities and list contact information:

2. Are there any opportunities to coordinate with relevant active private development within the project area? Yes No

Discuss private development coordination opportunities and contact information:

4 Street Classification & Type

Arterial Classification: Principal Minor Collector
 Non-Arterial Boulevard SFD Non-Arterial

If project area has multiple arterial classifications, describe:

<p>Street Types:</p> <ul style="list-style-type: none"> Alley Downtown Downtown Neighborhood Downtown Neighborhood Access Industrial Access Minor Industrial Access Neighborhood Corridor 	<ul style="list-style-type: none"> Neighborhood Yield Parks Boulevard Urban Center Connector Urban Village Main Urban Village Neighborhood Urban Village Neighborhood Access 	<p>Other Facilities:</p> <ul style="list-style-type: none"> Trails Unopened Right of Way Non-SDOT Property
--	--	---

If project area has multiple street types, please list which segments per type:

ROW Width:

Describe relevant standards from [Streets Illustrated](#) and any [deviations](#) you'll be requesting:

5 Safety & Channelization

1. [Posted Speed](#):
2. [85th percentile speed \(if available\)](#):
Location, date collected:
 - a. *Is the 85th percentile over posted speed?* Yes No
 - b. Are there high collision locations in the project area? Yes No
 - c. Are there Bicycle and Pedestrian Safety Analysis priority locations in the project areas? Yes No
 - d. Does the frequent Transit Network or RapidRide network operate in the project area? Yes No

If Yes to a, b, or c contact Vision Zero to discuss traffic calming recommendations. If yes to d. contact Transit and Mobility to discuss.

Describe recommendations:

4. a. [Average Weekday Traffic \(AWDT\)](#):
Location, date collected:
- c. [Average Weekday Traffic \(AWDT\)](#):
Location, date collected:
- b. [Average Weekday Traffic \(AWDT\)](#):
Location, date collected:

Describe final decision:

5. Does the project area have 4 or more lanes? Yes No
6. If AWDT is less than 25K and lane configuration includes 4 or more through lanes, contact Traffic Operations for review for potential rechannelization. If along RapidRide (existing or future), Priority Bus Network, or Frequent Transit Network include Transit and Mobility in these discussions.

Should rechannelization be considered in the project scope?

6 Pavement Condition

1. Is the Pavement Condition Index 65 or below at any point in the project area? Yes No

2. Describe any visible areas of disrepair in the roadway:

4. Describe any areaways in the project area:

Describe recommendations:

Please provide planning level cost estimates for recommendations:

Describe final decision:

7 Flex Lane / Curbspace

1. Will project change existing flex lane use(s)? Yes No
If No, skip to #7. If known, describe proposed changes:

2. Describe existing flex zone use(s) (e.g., loading zones) in project area:

3. Describe adjacent land use(s) that utilize the flex lane:

Residential
Commercial + Mixed Use
Industrial

4. Describe [ROW Allocation Framework](#) prioritized functions for the flex lane for specified land use(s) [in your project area](#):

5. What is the utilization of existing parking (e.g., peak parking occupancy)?

6. How can flex lane functions be met nearby or off-street?

7. Will any existing accessible parking spaces be impacted? Yes No

8. How many accessible on-street parking spaces is your project required to install? ([per Streets Illustrated section 3.13](#))

Describe recommendations for flex lane:

Describe final decisions:

8 Signals & Intelligent Transportation Systems (ITS)

1. Does the project include or impact traffic signals that are on the left-turn Signal List, the High Priority (new) Signal List, or the Major Maintenance (rebuild) List?

2. Is a full signal warranted in the project area? Yes No
If yes, consult with signal design manager about opportunities to upgrade.

3. Does the project area include any signals with a Condition Index read as the worst 10% of all signals?

4. Is the project on the ITS Key Arterial Network? Yes No
If so, list segments:

Describe recommendations:

Please provide planning level cost estimates:

Describe final decisions:

9 Pedestrian Infrastructure

- | | | | |
|----|--|-----|----|
| 1. | Is sidewalk repair needed in the project area?
If yes, contact the Sidewalk Safety Repair Program (SSRP) Manager. | Yes | No |
| 2. | Will sidewalk repair impact trees?
If yes, summarize recommendations from Urban Forestry: | Yes | No |
| 3. | Are there missing sidewalks in the project area?
If yes, contact the PMP Implementation Coordinator | Yes | No |
| 4. | Are there missing curb ramps or tactile pads in the project area?
If yes, contact ADA Program Manager | Yes | No |
| 5. | Are there Accessible Pedestrian Signal requests in the project area? If yes, contact ADA Program Manager | Yes | No |
| 6. | Is the project within a 20mph school zone or at a school crosswalk? If yes, contact SRTS Program Manager | Yes | No |
| 7. | Are there tier 1 or tier 2 signalized intersections in the project area? If yes, contact Pedestrian Crossing Lead | Yes | No |
| 8. | Are there tier 1 or tier 2 unsignalized intersections in the project area? If yes, contact Pedestrian Crossing Lead | Yes | No |
| | Describe tier 1 and tier 2 signalized & unsignalized recommendations: | | |
| 9. | Describe any adverse impacts to pedestrian travel triggered by your project (e.g., removal of a pedestrian buffer): | | |

Describe recommendations:

Please provide planning level cost estimates:

Describe final decisions:

10 Bicycle Infrastructure

- | | | | |
|----|--|-----|----|
| 1. | Does the project area contain locations on the Recommended Bicycle Network? | Yes | No |
| 2. | Is there an existing bike facility?
If yes, list street segments: | Yes | No |
| 3. | Do facilities in the project area meet the existing Bike Master Plan (BMP) designation?

<i>If existing facilities do not meet BMP designation, review Streets Illustrated for bicycle design guidance and consult with BMP Coordinator about opportunity to upgrade the facilities.</i> | Yes | No |
| 4. | Describe any adverse impacts to bicycle travel triggered by your project (e.g., bike lane closure during construction, pavement seam in bike lane, etc): | | |

Describe recommendations:

Please provide planning level cost estimates:

Describe final decision:

11

Transit Infrastructure

Question	Yes	No	<i>Describe recommendations:</i>
<p>1. Is there a bus route/bus stop/bus layover within the project area?</p> <p>If Yes, describe and consult Streets Illustrated for transit design standards. List them here.</p>			
<p>2. Is there a RapidRide (existing or future), Priority Bus Network, or Frequent Bus Network route within the project area?</p> <p>If Yes, describe which bus routes and type of overlap. Consult Transit Master Plan for investment recommendations. List recommendations here and consult with the Transit and Mobility group.</p>			<i>Please provide planning level cost estimates:</i>
<p>3. Is there overhead catenary wire for trolley buses within the project area?</p> <p>Is a change to channelization proposed with this project?</p> <p>If Yes to either of the above, describe and consult with the Transit and Mobility group who will connect you with the appropriate Metro contact, if necessary.</p>			Describe final decision:
<p>4. Are there transit stops in the project area more than 500 ft from a controlled crossing in the project area? Is there an opportunity to consolidate bus stops? List recommendations here and consult with the Transit and Mobility group who will connect you with the appropriate Metro contact, if necessary.</p>			
<p>5. Describe any adverse impacts to transit operations triggered by your project (e.g., any anticipated operational impacts to bus travel times, rechannelization, bus stop impacts etc.)</p>			

12 Freight Infrastructure

- | | | | | | | | | | |
|-----------------------------|---|--------------------|-----------------------|--------------------|------------------|-----------------------------|------------|--|--|
| 1. | Is the project on the Recommended Freight Network? | Yes | No | | | | | | |
| | <table border="0" style="width: 100%;"> <tr> <td style="width: 50%;">Major Truck Street</td> <td style="width: 50%;">Limited Access Street</td> </tr> <tr> <td>Minor Truck Street</td> <td>Over-Legal Route</td> </tr> <tr> <td>First / Last Mile Connector</td> <td>Heavy Haul</td> </tr> </table> | Major Truck Street | Limited Access Street | Minor Truck Street | Over-Legal Route | First / Last Mile Connector | Heavy Haul | | |
| Major Truck Street | Limited Access Street | | | | | | | | |
| Minor Truck Street | Over-Legal Route | | | | | | | | |
| First / Last Mile Connector | Heavy Haul | | | | | | | | |
| 2. | Does project area meet curb radius and clearance standards ? | Yes | No | | | | | | |
| 3. | Are there identified freight projects in project area? (Freight Master Plan (FMP)) | Yes | No | | | | | | |
| 4. | Is this project in the downtown traffic control zone ? | Yes | No | | | | | | |

Describe recommendations:

Please provide planning level cost estimates:

Describe final decision:

13 Urban Forestry

1. Describe any existing [urban forestry](#) assets within the project limits that need to be protected during construction:

Describe recommendations:

2. Are there Heritage Trees in the project area? Yes No

3. Does your project propose planting trees or expanding the ground plane landscape? Yes No

4. Will there be ground cover that requires maintenance or pruning? Yes No

5. Will sidewalk infrastructure be impacted (e.g. narrowing of sidewalks, sawcutting etc.)? Yes No

Please provide planning level cost estimates:

Describe final decision:

If yes, contact the Sidewalk Safety Repair Program (SSRP) Manager and summarize recommendations from SSRP:

14 Urban Design and Planning

<p>1. Is there a Street Design Concept Plan for the project area?</p> <p>2. List any plan(s) that overlap with project area (and relevant plan boundaries):</p>	Yes	No	<i>Describe recommendations:</i>
<p>3. Have other urban design or transportation plans been completed, or are draft plans in progress, within project area (including plans from other City departments)?</p>	Yes	No	
<p>4. Is there an opportunity to add pedestrian lighting in the project area?</p>	Yes	No	
<p>5. Is your project within the Age-Friendly Prioritization Area?</p>	Yes	No	<i>Please provide planning level cost estimates:</i>
<p>If yes, please contact Urban Design Program Coordinator, Policy and Planning to discuss opportunities for incorporating Age-Friendly Street Design elements.</p>			<i>Describe final decision:</i>
<p>The Age-Friendly Street Design elements may include:</p>			
<ul style="list-style-type: none"> • Seating • Public Toilets • Hill Climb Assists • Weather Protection • Wayfinding • Transit Amenities 			
<p>6. Is your project likely to include any departure from Streets Illustrated design standards and/or Best Management Practice e.g. two-way PBLs, interim design treatments - flexible delineators and paint striping for bike lanes, curb bulbs, alternative sidewalk designs, etc.?</p>	Yes	No	To be determined.
<p>If yes, please contact Urban Design Program Coordinator, Policy and Planning.</p>			

15 On-Site Stormwater Management

<p>1. Does your project create or replace 2,000 SF of hard surface, or disturbing 7,000 SF of land? If yes to either, do an early draft of drainage memo to better understand requirements</p>	Yes	No	<p><i>Describe recommendations:</i></p>	
<p>If no, skip to item 3.</p>				
<p>2. Have the minimum requirements of the 2016 Stormwater Code been evaluated?</p>	Yes	No	<p><i>Please provide planning level cost estimates:</i></p>	
<p>i. Is this project in an area identified as suitable for infiltrating GSI approaches (per SPU GIS data), including permeable pavement options?</p>	Yes	No		
<p>ii. Does project area require infiltration investigation? If investigation has been done, include findings in description of BMPs below</p>	Yes	No		
<p>iii. Are there opportunities in the project limits to accommodate On-Site Stormwater Management BMPs?</p>	Yes	No		
<p>iv. Is there an opportunity to remove impervious surface as part of this project in accordance with the 2013 Executive Order which urges all City departments to incorporate natural drainage features into capital projects?</p>	Yes	No		
<p><i>Please describe opportunities:</i></p>		<p>To be determined.</p>		<p>Describe final decision:</p>
<p><i>Please provide rough cost estimates:</i></p>				
<p>3. Is this project on a street identified as potentially eligible for SPU partnership opportunities (per SPU GIS data)?</p>	Yes	No		

Art

Consult with SDOT Arts & Enhancements Project Manager to complete this section.

Seattle was one of the first cities in the United States to adopt a percent-for-art ordinance in 1973. The program specifies that 1% of eligible city capital improvement project funds be set aside for the commission, purchase and installation of artworks in a variety of settings.

- | | | |
|---|-----|----|
| 1. Is there an opportunity for a 1% Percent for Art funded public art project(s) in the project area? | Yes | No |
| 2. Consult the SDOT Art Plan . Is there an opportunity to implement SDOT Art Plan toolbox elements (e.g. signal box art, sidewalk inlays, creative street furniture or bollards or planters, creative bicycle racks, etc.) in the project area? | Yes | No |

Contact: Kristen Ramirez
Email: kristen.ramirez@seattle.gov
Phone: (206) 615-1095

Prepare the following information:

1. Name of Program (official CIP name)
2. Approximate project scope & budget
3. Timing/schedule
4. Whether there is space for art in the project area

Describe final decisions:

Describe Public Art or SDOT Art Plan opportunities:

Based on the initial project information provided, the above noted Complete Streets elements are recommended to be incorporated into the project scope. The program owners and subject matter experts (collectively the Complete Streets Checklist Reviewers), who provided input through the Complete Streets Checklist process, will collectively make final decisions regarding project scope, based on these preliminary Complete Streets recommendations. If at any time, resolution between the team members cannot be reached regarding a scope item or additional department wide policy guidance is needed, the project should present the issue to the Complete Streets Steering Committee (CSSC).

In addition to these broad preliminary scope recommendations, ongoing urban design review is required for 30%, 60%, and 90% design drawings to review consistency with these preliminary recommendations, as well as ongoing design details and urban design opportunities. To the greatest extent possible, all major scope recommendations will be made during the Project Definition phase.

Should any scope changes be proposed post the Project Definition phase, the Project Manager is to inform the Complete Streets Checklist Reviewers (or CSSC if applicable) and obtain consensus for the revised scope. The Complete Streets Checklist and Project Definition Memo will need to be updated accordingly.

Project Developer

_____ *name (please print)*

_____ *date*

_____ *signature*

Project Manager

_____ *name (please print)*

_____ *date*

_____ *signature*

Appendix B. Traffic Analysis Memorandum

Date: May 20, 2020

To: Vanessa Bacurin, Project Manager
Seattle Department of Transportation

From: Charlie Wence, Jacobs

CC: Mark Johnson, PE, Jacobs

Subject: Traffic Analysis Report – Admiral Way North and South Bridges

INTRODUCTION

The Seattle Department of Transportation is retrofitting the Admiral Way North and South Bridges (Admiral Way Bridges) to withstand future earthquakes. The purpose of this traffic analysis report is to evaluate temporary traffic impacts associated with the seismic retrofit construction of the bridges.

The following construction activities are proposed for this project:

- Retrofitting of both the North and South bridges by addressing deficiencies at the abutments, interior pier bents, bent footings, and expansion joints
- Strengthening the longitudinal joint of the bridge deck at the roadway level
- Performing ground improvement of the steep slopes under the bridge to prevent slope stability failure in a seismic event.

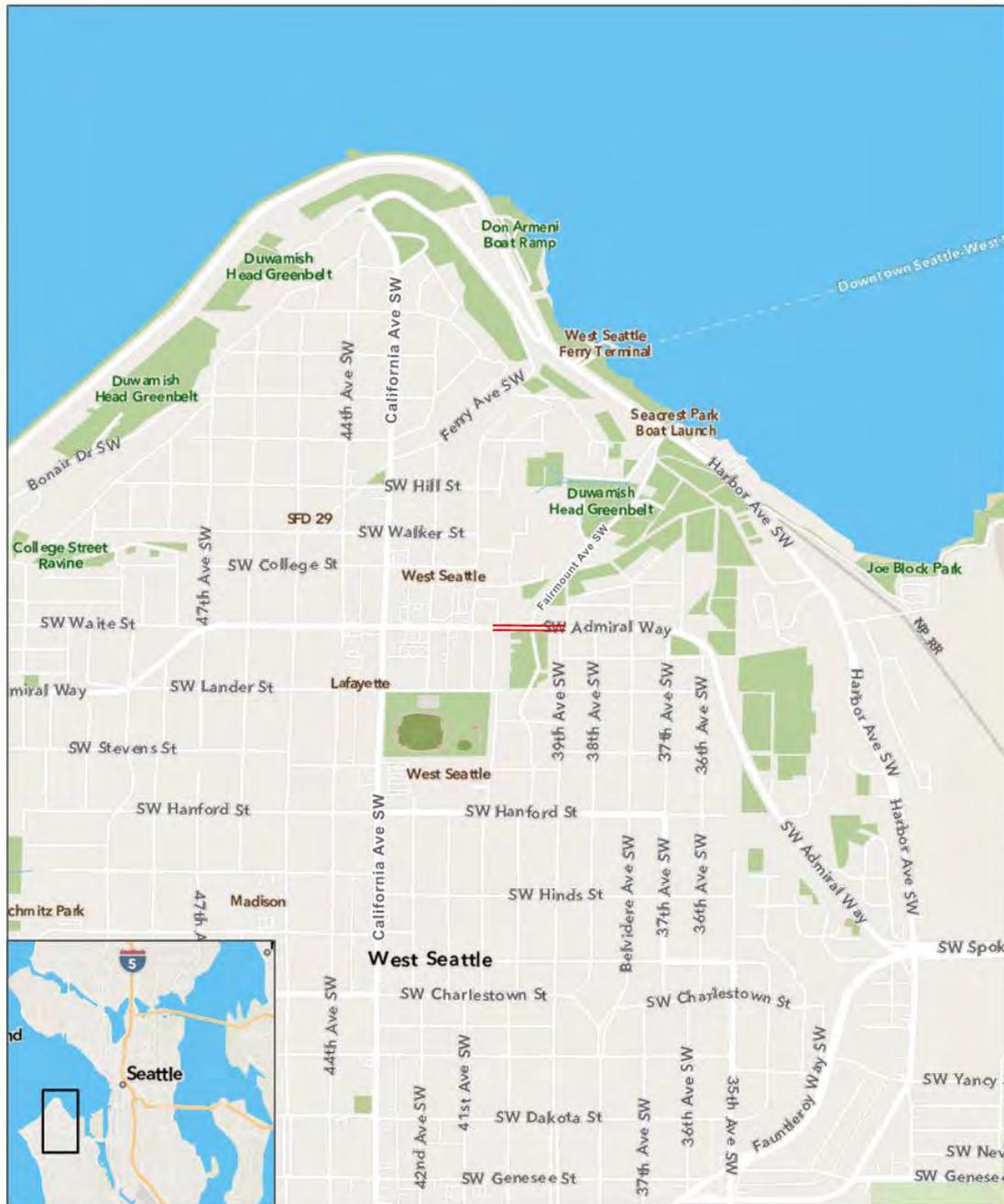
These proposed construction activities could impact travelers in and around the work zone due to temporary bridge closures or restrictions to pedestrian and bicycle routes. This will require considerations for maintaining auto, truck, and transit traffic and pedestrian and bicycle access in the project vicinity.

EXISTING ROADWAY CONDITIONS

Admiral Way Bridges

The two bridges are located between Walnut Avenue SW and 39th Avenue SW in Seattle's North Admiral neighborhood. The bridges are approximately 290 feet long and span a ravine that is traversed by Fairmount Avenue SW, which connects SW Hanford Street to Harbor Avenue SW. The two bridges consist of distinct structures built at different times that are connected at the roadway deck level. The North Bridge was constructed in 1927 and is a cast-in-place concrete seven-span structure with a concrete truss. The South Bridge was constructed in 1949 and is a steel girder five-span structure. The bridges were retrofitted in the mid-1990s to connect the two bridges along the longitudinal roadway deck joint. The total width of the bridges is 63 feet and they carry a four-lane, 48 foot roadway with 6 ft sidewalks on each side.

Figure 1 shows the location of the Admiral Way bridges.



— Admiral Way Bridges
 □ Map Extent

0 0.25 0.5 Miles



Sources: Esri, HERE, Garmin, FAO, NOAA, USGS, © OpenStreetMap contributors, and the GIS User Community

Figure 1
Project Vicinity

SW Admiral Way

This street connects the North Admiral neighborhood to the West Seattle Bridge. Near the bridges, SW Admiral Way has two lanes in each direction with either a two-way left turn lane or a raised grass median. There are no shoulders and on-street parking is not provided. The existing posted speed limit is 30 mph.



A view of the Admiral Way SW bridges looking west

Fairmount Avenue SW

This street connects residential areas of the North Admiral neighborhood to Alki Beach. Fairmount Avenue SW runs the length of the ravine that the Admiral Way bridges span over and provides access to private driveways north of the bridge at Harbor Avenue SW. Within the ravine, the roadway is narrow at approximately 16 ft in width with no sidewalks or shoulders. On-street parking is not provided. The posted speed limit is 20 mph.



Fairmount Avenue SW looking south towards the bridges

PEDESTRIAN AND BICYCLE FACILITIES

There are 6 foot sidewalks separated by concrete barrier on both sides of the Admiral Way bridges. The closest marked crosswalk is approximately 150 ft west of the bridge at the intersection of 41st Avenue SW/SW Admiral Way. There is no parking or shoulder area on the bridges. Bicycle sharrow markings are striped in both directions approaching the bridges.



Concrete barrier protected sidewalks on bridges looking east

TRANSIT ACCESS

Transit service is provided by King County Metro Routes 56 (Alki to Downtown Seattle) and 57 (Alaska Junction to Downtown Seattle) at stops on SW Admiral Way on both sides of the bridges. Routes 56 and 57 share the same bus stops and are located west of the bridges at SW Admiral Way and 41st Avenue SW and east of the bridges at SW Admiral Way and 38th Avenue SW.



Route 56/57 Bus Stop and Map

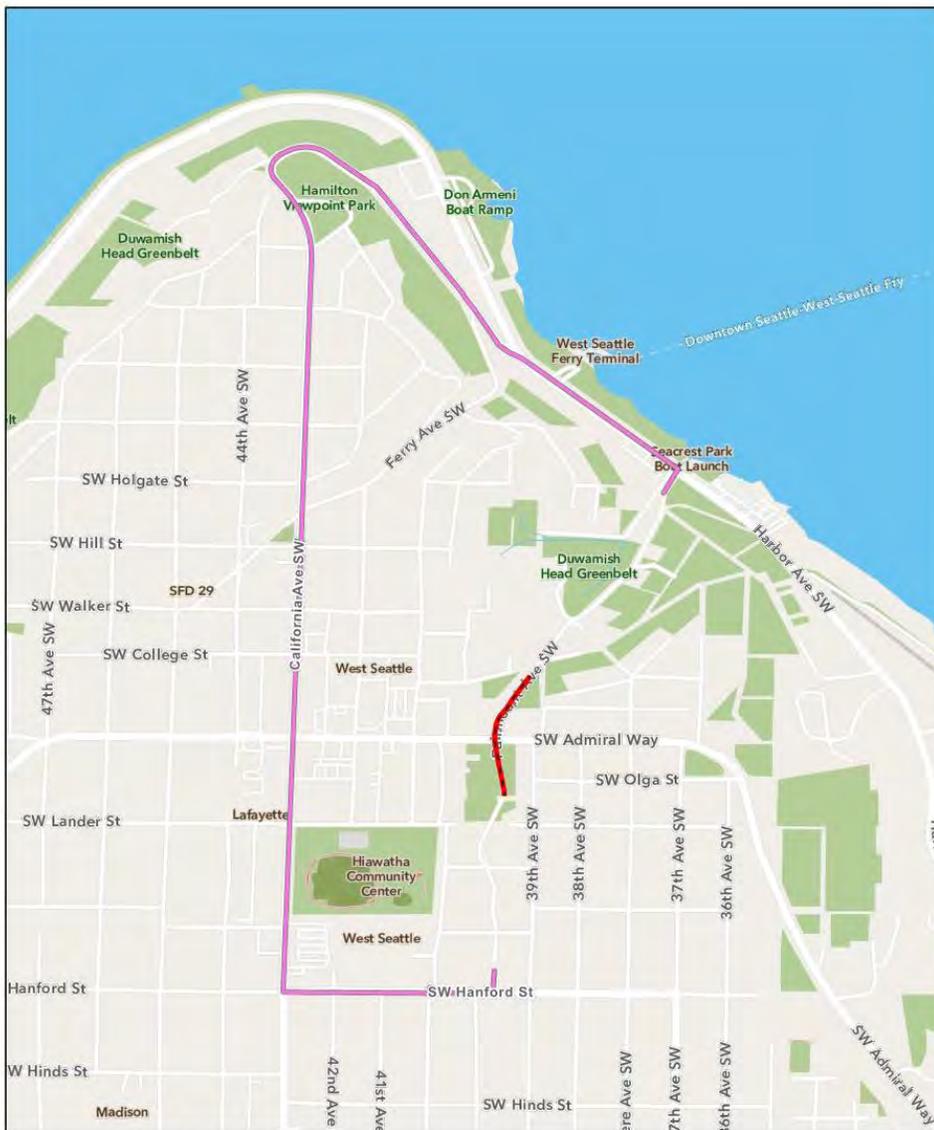
These routes operate directionally during weekday morning and afternoon commuting hours (to Downtown Seattle during mornings and from Downtown Seattle during afternoons). No weekday off-peak or weekend service is provided for Routes 56 and 57. If traffic in both directions can be maintained during construction for the peak commuting hours then no bus re-reroute will be necessary. SDOT will need to coordinate with King County Metro if construction requires closing the bridges during weekday peak hours.

PRIVATE PROPERTY ACCESS

Construction access and staging will primarily occur in the ravine below the bridges on Fairmount Avenue SW. Access to private driveways on Fairmount Avenue SW north of the bridges will need to be maintained throughout the duration of construction. Should construction equipment and activity require closing Fairmount Avenue SW underneath the bridges, access to the private property driveways to the north will only be possible via Harbor Avenue SW.

CONSTRUCTION TRAFFIC REQUIREMENTS

Construction staging will primarily occur below the bridges on Fairmount Avenue SW. Closing this street to traffic would require a detour to other north-south streets connecting the North Admiral neighborhood and Alki, such as California Avenue SW. Figure 2 shows the proposed detour route for a Fairmount Avenue SW closure.



Fairmount Ave Detour Route
 — Fairmount Ave (Closed)
 — Fairmount Ave Detour Route

0 0.13 0.25 Miles

Sources: Esri, HERE, Garmin, FAO, NOAA, USGS, © OpenStreetMap contributors, and the GIS User Community

Figure 2
 Fairmount Ave Detour Route

Travel lane and sidewalk closures on the bridges will be needed during construction and the Contractor will be required to provide for safe public access and travel through the work zone. It is expected that up to two of the four lanes and one of the sidewalks will need to be used for construction staging and retrofit construction at various times during construction. The work zone areas could include the south sidewalk and two adjacent travel lanes, the middle two lanes of the bridge, or the north sidewalk and adjacent two travel lanes. Temporary barriers will be placed to separate the work zone from the travelling public.

In addition, there will likely be times when the entire bridge needs to be closed during certain construction activities. Vehicular traffic, pedestrians, and bicyclists will need clearly marked detour routes around the work zone. Since SW Admiral Way is a vital link between the North Admiral neighborhood and the West Seattle Bridge and downtown Seattle, potential detours would require accessing California Avenue SW from either Harbor Avenue SW to the north or SW Oregon Street to the south. In addition, a temporary relocation of bus stops and reroute plan and would be needed for Routes 56 and 57.

Figure 3 shows the proposed detour routes for a temporary closure of the Admiral Way Bridge.

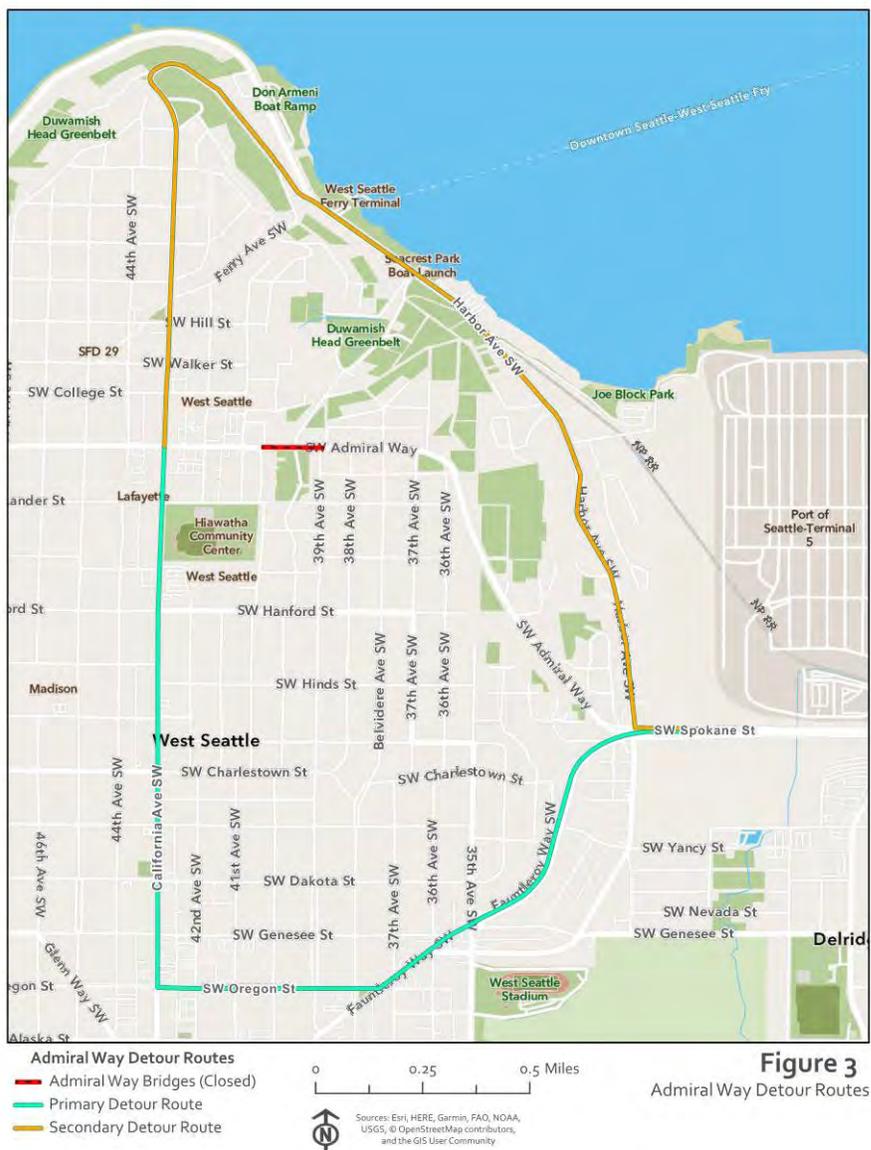


Figure 3
Admiral Way Detour Routes

Appendix C. Stormwater Management Flowchart

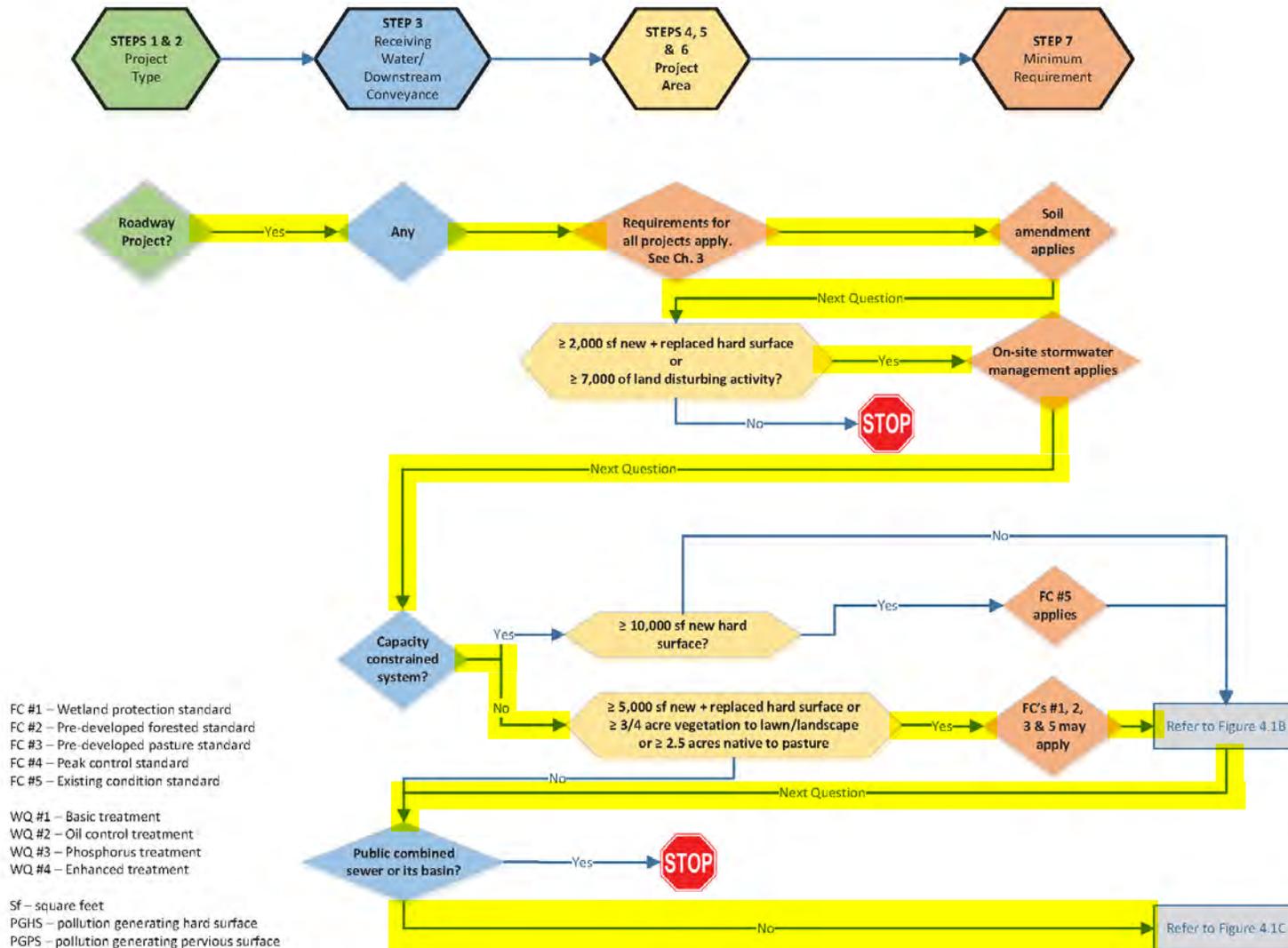


Figure 4.1A. Project Minimum Requirements for Roadway Projects.

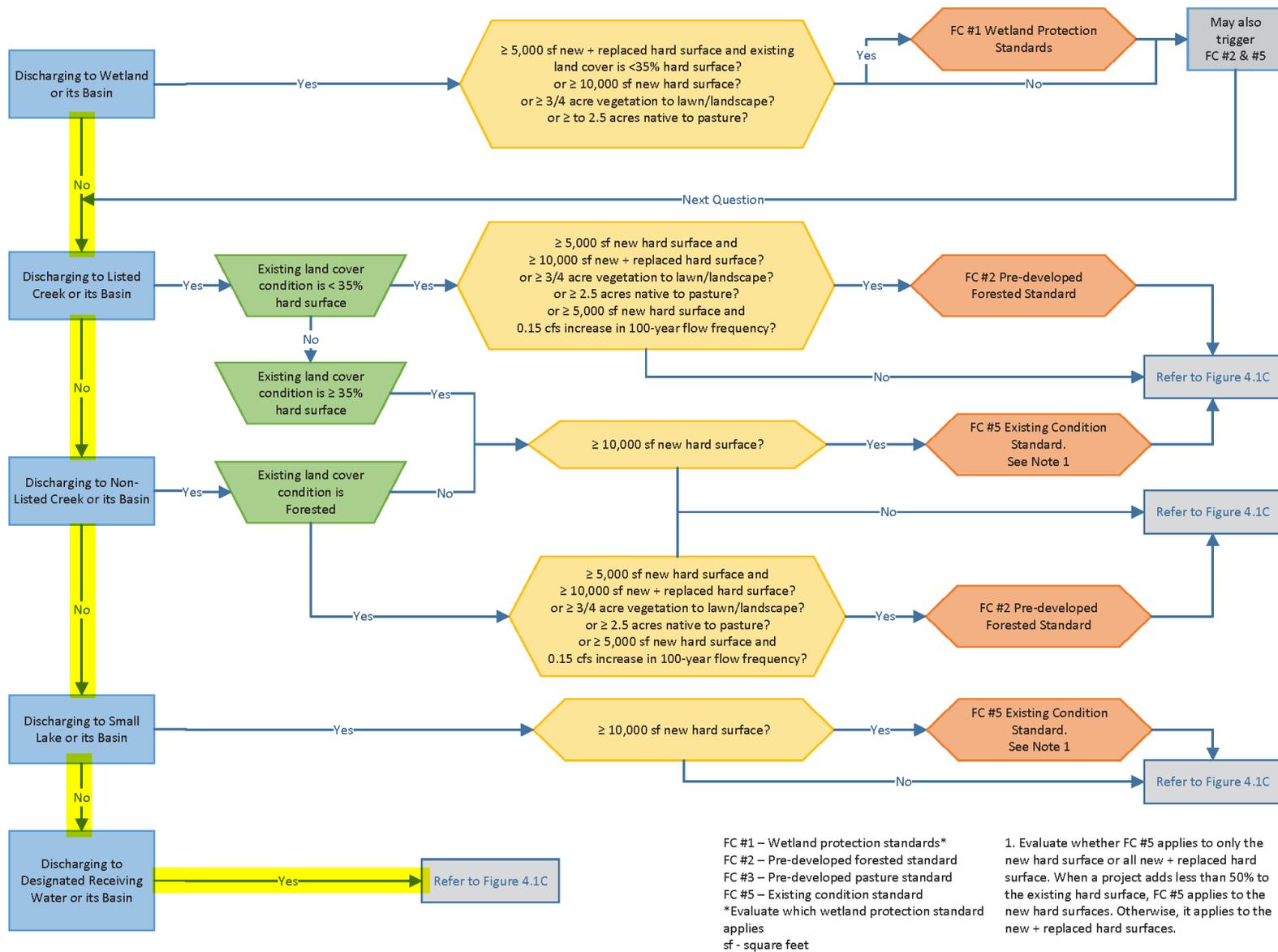


Figure 4.1B. Project Minimum Requirements for Roadway Projects (continued).

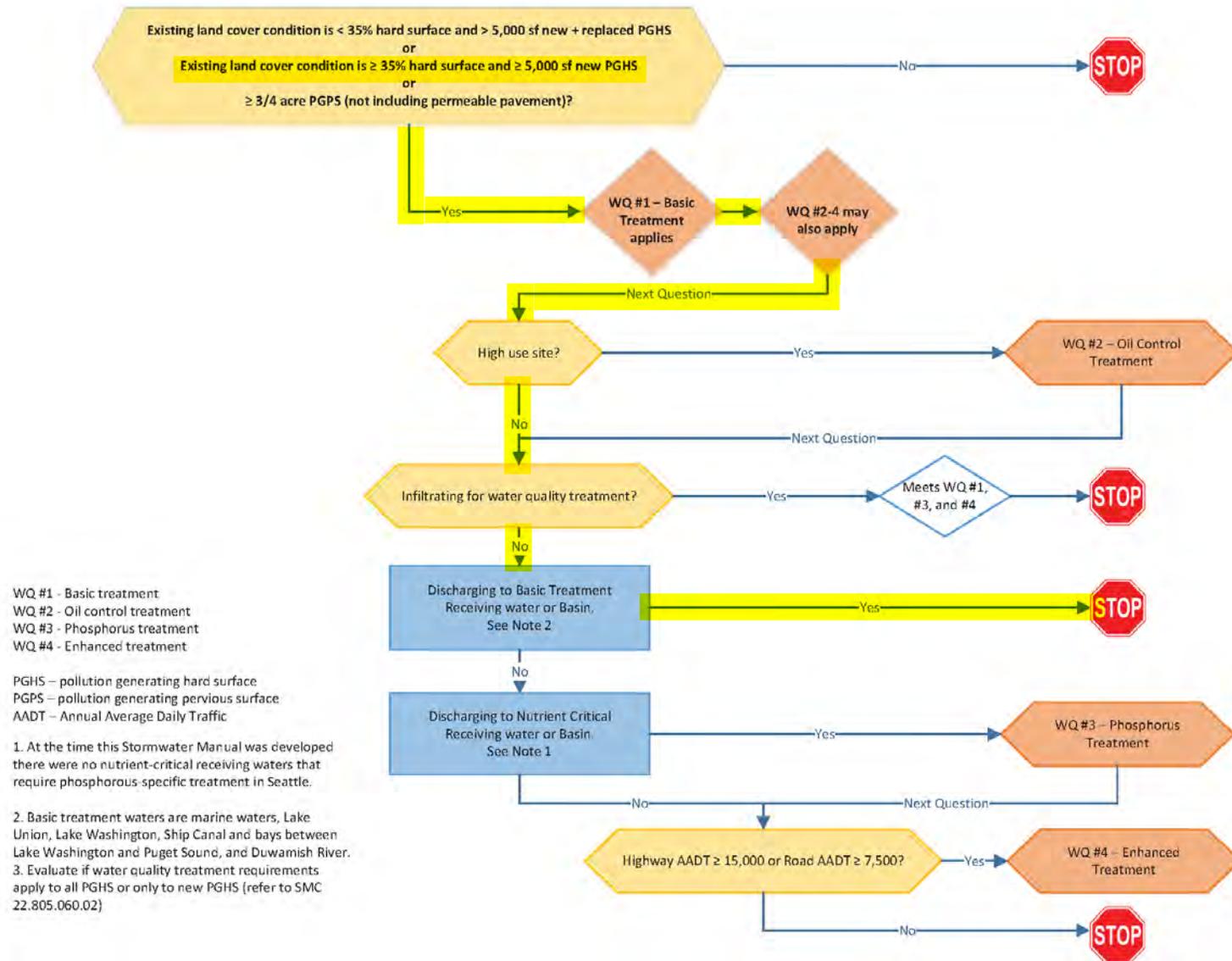


Figure 4.1C. Project Minimum Requirements for Roadway Projects (continued).

Appendix D. Concept-Level Cost Estimate



May 9, 2024

To: Mark Johnson – Jacobs

From: David Doles – Ott-Sakai and Associates

Re: Admiral Way Bridge Replacement Study

Ott Sakai and Associates has been tasked to perform high level estimates and schedules for both the alternatives for replacement of the Admiral Way Bridge. To keep everything current, I will restate much of the information that was included in our memo of Oct 5, 2022.

Alternative 1 would have complete closure of Southwest Admiral Way allowing the contractor to complete the entire new structure without having to accommodate traffic in the work area.

Alternative 2 involved demolition and new construction of one of the two existing bridges while maintaining two-way traffic on the remaining existing bridge. Then, with traffic moved to the new construction, the remaining structure would be replaced. In this iteration an additional constraint was requested, in that the previously recommended take of the 4 corner properties was not acceptable.

Items Common to Both Alternatives

- We have assumed WF42G precast concrete girders. Thus Spans 1 and 3 girder weights are estimated at 89326 lbs while Span 2 is estimated at 76442 lbs.
- We have assumed shaft depths to be 75 feet.
- The estimate does not deal with the existing gas line on the South Bridge and assumes it to have been relocated or abandoned.
- We have assumed that new slope stabilization measures would be needed..
- We assume that noise ordinances will place limitations on any night shift work.
- A drill pad will have to be constructed on Fairmount for accessing the drill equipment to the shafts. Fairmount appears to be on a 9.5% downslope, and we estimate a temporary fill amount of about 1800 cy for the pad. It appears that this pad can be constructed within the existing R/W.
- TCE's 30 feet wide, from the abutments downslope to the Fairmount R/W on both the North and South sides, will be necessary for the demo equipment to access the existing North and South structures.
- Access to the shafts and substructure at Piers 2 and 3 would be a work zone created by closing Fairmount from SW Lander, north to the first residential Fairmount driveway that is north of the bridge.
- Both estimates use 2024 pricing.
- Both estimates include 20% markup for Overhead and Profit as well as contingencies for design and permitting, construction, construction engineering and Right of Way



Alternative 1

- With the full closure of Admiral Way, we believe the work zone at the deck level should be the full width of Admiral Way and extend from the eastern edge of 41st Street to the western edge of the driveway servicing the residence at 3810 Southwest Admiral Way. This would include closing access from Admiral Way to Walnut and 39th. Staging areas created would be roughly 15,000 sf to the west and 12,000 sf to the east.
- We believe that demolition of the existing structures can be accomplished with conventional demo equipment that is readily available in the greater Seattle area. As noted above, it will require the TCE access on slopes north and south of the bridges.
- For girder sets, if both existing structures have been removed, a 500T class crane at the abutment can pass one end of the girder to another 500T staged on the pad on Fairmount and then hoist the remaining end from the transporter to set. This can be repeated for all three spans. This is the assumed method for girder sets in the schedule we have prepared.
- As an alternative girder set scheme, the contractor may elect to demo and construct only one existing bridge at a time (similar to Alternative 2) utilizing the remaining structure to deliver the girders to 200T-250T class cranes staged at the abutments and again on the pad on Fairmount. This would obviate the need to pass one end of the girders from one crane to another. Similarly, once the first stage new bridge is in place it can be used to deliver girders for the second stage. This scheme would require additional mobes for drill and demo equipment but may be more attractive to a contractor from a cost and operational standpoint. Another negative is that it would add several months of schedule to the project.

Alternative 2

- Similar to Alternative 1, our recommended staging areas east and west of the bridge would be 30' wide and extend from 41st to the residential driveway at 3810 Southwest Admiral Way. Staging areas west and east would be roughly 7,500 sf and 6,000 sf respectively. This arrangement will require more traffic control efforts to effect material delivery since large trucks and semi traffic don't have enough room to turn around.
- The 30' wide work zone at the abutments severely restricts operations of larger equipment. The narrow width makes passing movements difficult while handling materials. For example, a 65T RT Grove Crane barely has the room to set up and be able to swing suspended material east to west at its minimum radius without passing the materials over either live traffic or the adjacent properties. A Linkbelt 248H Crawler Crane with 120' boom requires a minimum of 38' width to be able to make a similar movement and thus will not work with the 30 ft width. To overcome these restrictions its possible to set up a crane with a luffing boom configuration on a pad on Fairmount. However, it still has a very real risk of accidentally swinging suspended loads over live traffic. See Annex 1 below.
- For Girder sets, utilize the remaining structure on night shift closures to deliver the girders to 200T-250T class cranes staged at the abutments and on the pad on Fairmount. Once the first



stage new bridge is in place it can be used to deliver girders for the second stage. This scheme requires additional moves for drill and demo equipment

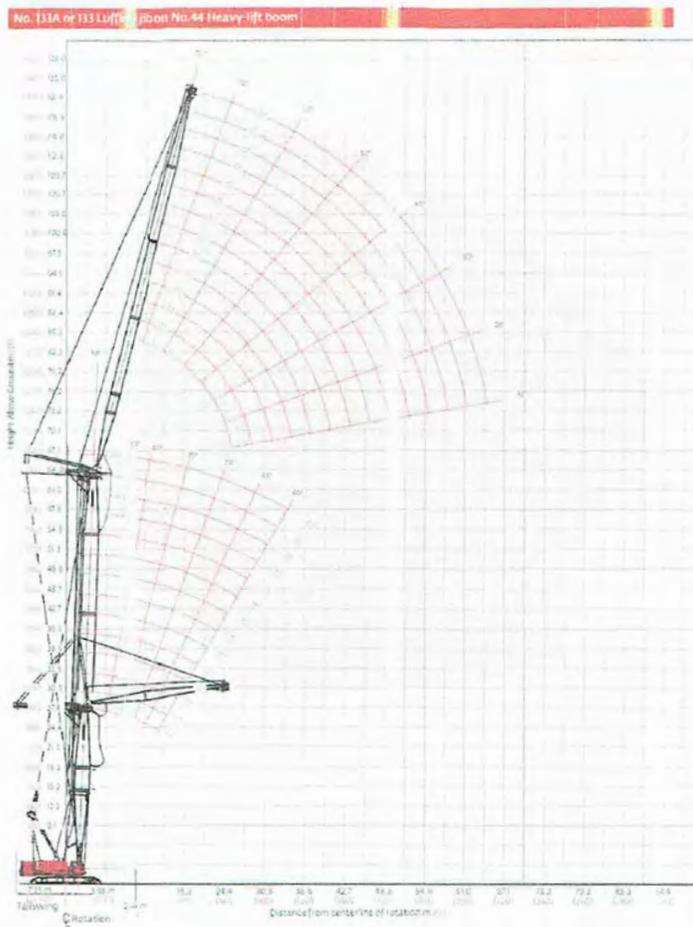
- As an alternative to the luffing crane on Fairmount, use similar night closures for materials that would otherwise be difficult to pass from the abutment to the deck (rebar, etc).

Conclusions

1. Constructing the project in a single phase with the full closure of Admiral Way can be accomplished in 10.5 months with the detour in place. (Initial TC closure to Open Bridge)
2. Constructing the bridge in two phases requires single lane traffic in both directions for a period of about 17.5 months. (Initial TC closure to Open Bridge)
3. Within those 17.5 months we estimate there would be 40 nights or days when full closures would be required for certain activities within the 2-phase option.
4. Constructing in two phases is roughly considerably more expensive than the single-phase option. The primary differences in cost are those associated with the extended time to perform, the extra moves and demoves for the specialty subs and their equipment and the increased level of traffic control required.

Annex 1

Luffing jib range diagram



Manitowoc 2250

41

Ott-Sakai & Associates, LLC
 Admiral Way Bridge Planning Study
 Date: 5/29/24

CONCEPT-LEVEL PROJECT COSTS - 64' WIDE REPLACEMENT BRIDGE

No.	Bid Item
1	CONSTRUCTION SURVEYING
2	TESC
3	MAINTENANCE AND PROTECTION OF TRAFFIC CONTROL INCL FLAGGING
4	SITE PREPARATION
5	DEMO EXISTING BRIDGES
6	TEMP CRANE PAD ON FAIRMOUNT
7	CONSTRUCTING 7' DIAM. SHAFT
8	SOIL NAILS
9	SUBSTRUCTURE CONCRETE
10	SUPERSTRUCTURE CONCRETE
11	BRIDGE OVERHEAD AND EQUIPMENT SUPPORT
12	ADDL MOBS & EQUIPMT - 2-PHASE CONSTR (DEMO, DRILLER, GIRDER SET)
13	DETOUR COSTS (TRAFFIC SIGNAL UPGRADES, CURB RAMPS, ETC)
14	SITE RESTORATION
15	LANDSCAPING
16	PROJECT ELECTRICAL

SINGLE STAGE CONSTRUCTION				
Quantity	Units	Unit Price	Price	Comments
1	LS	\$ 50,000	\$ 50,000	
1	LS	\$ 200,000	\$ 200,000	
10	MO	\$ 24,000	\$ 240,000	
1	LS	\$ 150,000	\$ 150,000	
18,872	SF	\$ 58	\$ 1,095,000	
1,800	CY	\$ 75	\$ 135,000	
900	LF	\$ 3,500	\$ 3,150,000	3 shafts/bent
40	EA	\$ 10,000	\$ 400,000	
540	CY	\$ 1,200	\$ 648,000	6 columns total
19,200	SF	\$ 165	\$ 3,168,000	
10	MO	\$ 150,000	\$ 1,500,000	
-	-	\$ -	\$ -	
1	LS	\$ 300,000	\$ 300,000	
1	LS	\$ 150,000	\$ 150,000	
1	LS	\$ 75,000	\$ 75,000	
1	LS	\$ 100,000	\$ 100,000	

TWO STAGE CONSTRUCTION				
Quantity	Units	Unit Price	Price	Comments
1	LS	\$ 50,000	\$ 50,000	
1	LS	\$ 300,000	\$ 300,000	
16	MO	\$ 48,000	\$ 768,000	
1	LS	\$ 200,000	\$ 200,000	
18,872	SF	\$ 58	\$ 1,095,000	
1,800	CY	\$ 75	\$ 135,000	
1,200	LF	\$ 3,500	\$ 4,200,000	4 shafts/bent
40	EA	\$ 10,000	\$ 400,000	
620	CY	\$ 1,200	\$ 744,000	8 columns total
19,200	SF	\$ 165	\$ 3,168,000	
16	MO	\$ 150,000	\$ 2,400,000	
1	LS	\$ 500,000	\$ 500,000	
-	-	\$ -	\$ -	
1	LS	\$ 150,000	\$ 150,000	
1	LS	\$ 75,000	\$ 75,000	
1	LS	\$ 100,000	\$ 100,000	

			\$ 11,361,000	(1)
	OVERHEAD AND PROFIT (MOBILIZATION)	20%	\$ 2,272,200	(2) = 20% of (1)
	TOTAL ROM CONSTRUCTION COST		\$ 13,633,200	(3)
	DESIGN & PERMITTING - CONSULTANT	20%	\$ 2,726,640	(4) = 20% of (3)
	DESIGN & PERMITTING - CITY ADMIN	15%	\$ 2,044,980	(5) = 15% of (3)
	CONSTRUCTION CONTINGENCY	50%	\$ 6,816,600	(6) = 50% of (3)
	CONSTRUCTION ENGINEERING	20%	\$ 2,726,640	(7) = 20% of (3)
	RIGHT OF WAY (ALLOWANCE)		\$ 500,000	(8) = allowance
	TOTAL ROM PROJECT COST		\$ 28,500,000	(9)

			\$ 14,285,000	(1)
	OVERHEAD AND PROFIT (MOBILIZATION)	20%	\$ 2,857,000	(2) = 20% of (1)
	TOTAL ROM CONSTRUCTION COST		\$ 17,142,000	(3)
	DESIGN & PERMITTING - CONSULTANT	20%	\$ 3,428,400	(4) = 20% of (3)
	DESIGN & PERMITTING - CITY ADMIN	15%	\$ 2,571,300	(5) = 15% of (3)
	CONSTRUCTION CONTINGENCY	50%	\$ 8,571,000	(6) = 50% of (3)
	CONSTRUCTION ENGINEERING	20%	\$ 3,428,400	(7) = 20% of (3)
	RIGHT OF WAY (ALLOWANCE)		\$ 2,000,000	(8) = allowance
	TOTAL ROM PROJECT COST		\$ 37,200,000	(9)

LOW ESTIMATE OF RANGE - 20% \$ 22.8 M
 HIGH ESTIMATE OF RANGE + 50% \$ 42.8 M

LOW ESTIMATE OF RANGE \$ 29.8 M
 HIGH ESTIMATE OF RANGE \$ 55.8 M

Appendix E. Constructability Evaluation



October 5, 2022

To: Mark Johnson – Jacobs

From: David Doles – Ott-Sakai and Associates

Re: Admiral Way Bridge Replacement Study

Ott Sakai and Associates has been tasked to review equipment layouts, potential staging areas and TCE requirements for the replacement of the Admiral Way bridges. We were asked to review two alternatives.

Alternative 1 would have complete closure of Southwest Admiral Way allowing the contractor to complete the entire new structure without having to accommodate traffic in the work area.

Alternative 2 involved demolition and new construction of one of the two existing bridges while maintaining two-way traffic on the remaining existing bridge. Then, with traffic moved to the new construction, the remaining structure would be replaced.

Items Common to Both Alternatives

- We have assumed WF42G precast concrete girders. Thus Spans 1 and 3 girder weights are estimated at 89326 lbs while Span 2 is estimated at 76442 lbs.
- A drill pad will have to be constructed on Fairmount for accessing the drill equipment to the shafts. Fairmount appears to be on a 9.5% downslope, and we estimate a temporary fill amount of about 1800 cy for the pad. It appears that this pad can be constructed within the existing R/W.
- TCE's 30 feet wide, from the abutments downslope to the Fairmount R/W on both the North and South sides, will be necessary for the demo equipment to access the existing North and South structures.
- Access to the shafts and substructure at Piers 2 and 3 would be a work zone created by closing Fairmount from SW Lander, north to the first residential Fairmount driveway that is north of the bridge.

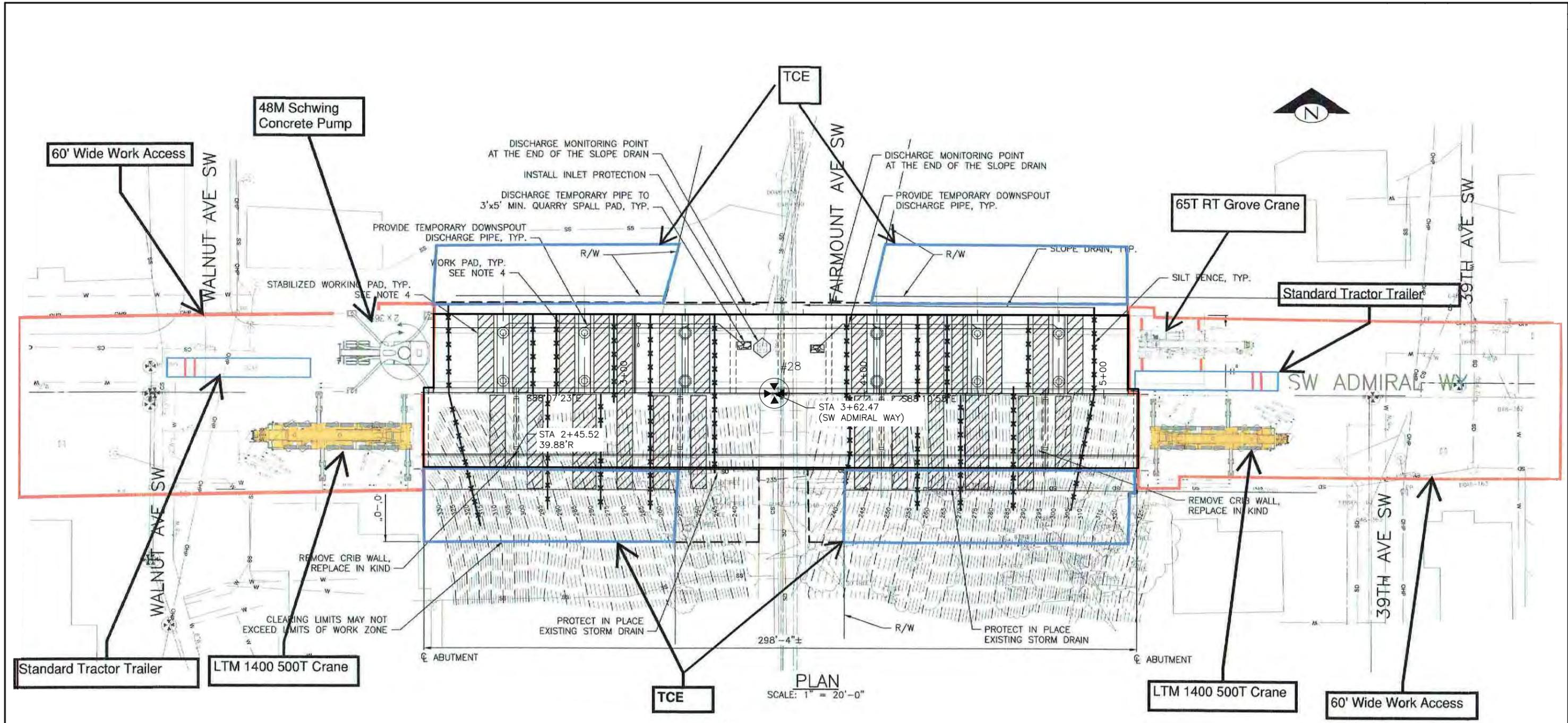
Alternative 1

- With the full closure of Admiral Way, we believe the work zone at the deck level should be the full width of Admiral Way and extend from the eastern edge of 41st Street to the western edge of the driveway servicing the residence at 3810 Southwest Admiral Way. This would include closing access from Admiral Way to Walnut and 39th. Staging areas created would be roughly 15,000 sf to the west and 12,000 sf to the east.

- We believe that demolition of the existing structures can be accomplished with conventional demo equipment that is readily available in the greater Seattle area. As noted above, it will require the TCE access on slopes north and south of the bridges.
- For girder sets, if both existing structures have been removed, a 500T class crane at the abutment can pass one end of the girder to another 500T staged on the pad on Fairmount and then hoist the remaining end from the transporter to set. This can be repeated for all three spans.
- As an alternative girder set scheme, the contractor may elect to demo and construct only one existing bridge at a time (similar to Alternative 2) utilizing the remaining structure to deliver the girders to 200T-250T class cranes staged at the abutments and again on the pad on Fairmount. This would obviate the need to pass one end of the girders from one crane to another. Similarly, once the first stage new bridge is in place it can be used to deliver girders for the second stage. This scheme would require additional mobes for drill and demo equipment but may be more attractive to a contractor from a cost and operational standpoint.

Alternative 2

- Similar to Alternative 1, our recommended staging areas east and west of the bridge would be 30' wide and extend from 41st to the residential driveway at 3810 Southwest Admiral Way. Staging areas west and east would be roughly 7,500 sf and 6,000 sf respectively. This arrangement will require more traffic control efforts to effect material delivery since large trucks and semi traffic don't have enough room to turn around.
- The 30' wide work zone at the abutments severely restricts operations of larger equipment. The narrow width makes passing movements difficult while handling materials. For example, a 65T RT Grove Crane barely has the room to set up and be able to swing suspended material east to west at its minimum radius without passing the materials over either live traffic or the adjacent properties. A Linkbelt 248H Crawler Crane with 120' boom requires a minimum of 38' width to be able to make a similar movement and thus will not work with the 30 ft width. For this reason, we believe that the acquisition of the 4 properties at the 4 corners of the structure should be seriously considered.
- For Girder sets utilize the remaining structure on night shift closures to deliver the girders to 200T-250T class cranes staged at the abutments and on the pad on Fairmount. Once the first stage new bridge is in place it can be used to deliver girders for the second stage. This scheme requires additional mobes for drill and demo equipment
- Use similar night closures for materials that would otherwise be difficult to pass from the abutment to the deck (rebar, etc).



ALTERNATIVE 1 - SINGLE STAGE CONSTRUCTION

LEGEND:

- POSSIBLE WORK PAD LOCATION
- LIMITS OF WORK ZONE
- RIGHT OF WAY LINE
- SILT FENCE

20 10 0 20 40
SCALE IN FEET

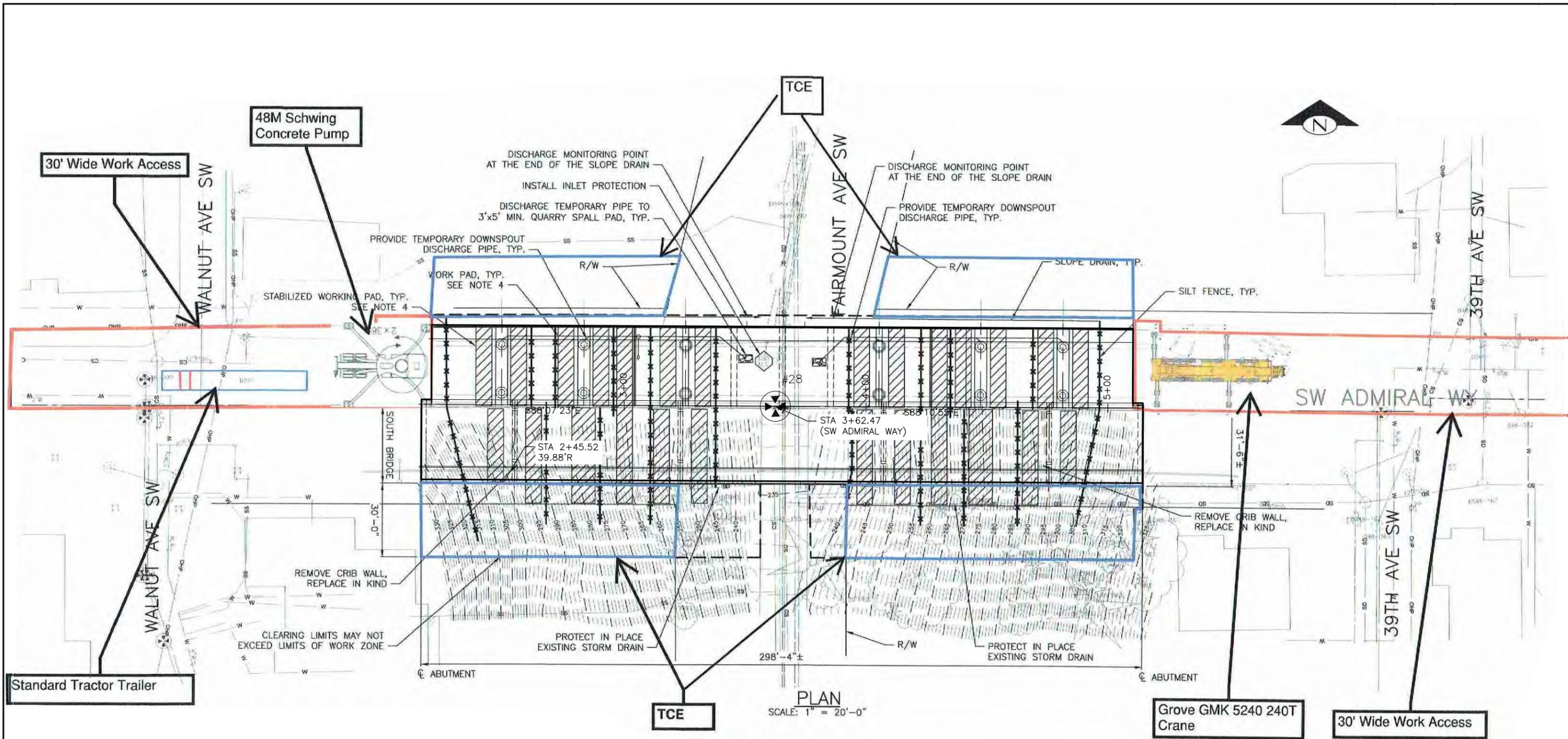
APPROVED FOR ADVERTISING		INITIALS AND DATE		INITIALS AND DATE	
DEPARTMENT OF FINANCE & ADMINISTRATIVE SERVICES SEATTLE, WASHINGTON 20		DESIGNED CHECKED		REVIEWED: DES. CONST. SDOT PROJ. MGR.	
BY:		DRAWN CHECKED		RECEIVED	
PURCHASING AND CONTRACTING		ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE CITY OF SEATTLE STANDARD PLANS AND SPECIFICATIONS AND OTHER DOCUMENTS CALLED FOR IN SECTION 0-02.3 OF THE PROJECT MANUAL.		REVISED AS BUILT	

Seattle Department of Transportation

SCALE: AS NOTED

ADMIRAL WAY BRIDGE REPLACEMENT STUDY

JOB #	
VPI #	



ALTERNATIVE 2 - TWO STAGE CONSTRUCTION

LEGEND:

- POSSIBLE WORK PAD LOCATION
- LIMITS OF WORK ZONE
- RIGHT OF WAY LINE
- SILT FENCE

20 10 0 20 40
SCALE IN FEET

APPROVED FOR ADVERTISING		INITIALS AND DATE		INITIALS AND DATE	
DEPARTMENT OF FINANCE & ADMINISTRATIVE SERVICES SEATTLE, WASHINGTON 20		DESIGNED CHECKED		REVIEWED: DES. CONST. SDOT PROJ. MGR.	
BY: PURCHASING AND CONTRACTING		DRAWN CHECKED		RECEIVED	
		ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE CITY OF SEATTLE STANDARD PLANS AND SPECIFICATIONS AND OTHER DOCUMENTS CALLED FOR IN SECTION 0-02.3 OF THE PROJECT MANUAL.			

Seattle Department of Transportation

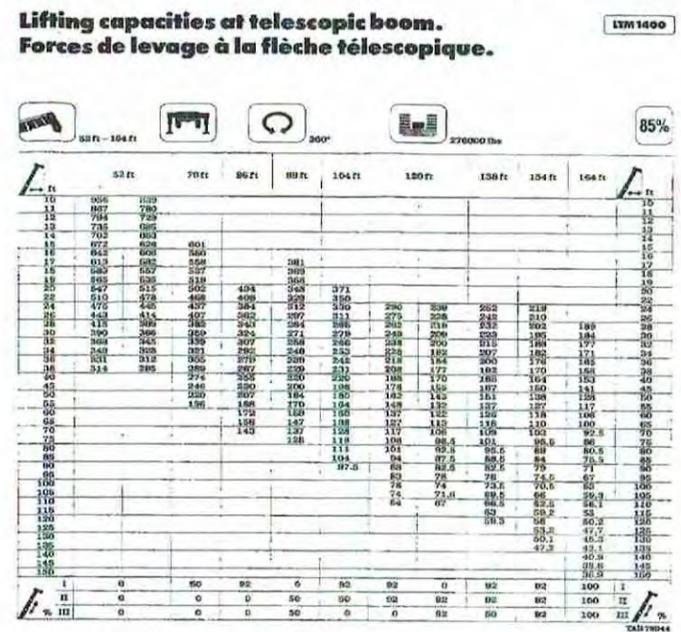
SCALE: AS NOTED

ADMIRAL WAY BRIDGE REPLACEMENT STUDY

JOB #	
VPI #	

BI	Bridge	Span	Girder	Girder	Max Length/Ea	Total Length	Lbs/LF	Lbs/Each	Each	Special	Duration	Crane	Girder Delivery from	Expected Delivery Month	Deduct Ea (lbs)	# Crane Pick	Crane Load Ea	Cranes w/ Counterweight													
																		#1	C.W. (lb)	Radius (ft)	Boom (ft)	Capacity	% Chart	#2	C.W. (lb)	Radius (ft)	Boom (ft)	Capacity	% Chart		
	Admiral Way	1		WF42G	104.00	832.00	858.9	89,326	8						4000	2	48,663	LTM-1400	276,000	72	164	78,000	62%	LTM-1400	276,000	83	154	85,000	57%		
	Admiral Way	2		WF42G	89.00	712.00	858.9	76,442	8						4000	2	42,221	LTM-1400	276,000	72	164	78,000	54%	LTM-1400	276,000	120	154	56,000	75%		
	Admiral Way	3		WF42G	104.00	832.00	858.9	89,326	8						4000	2	48,663	LTM-1400	276,000	72	164	78,000	62%	LTM-1400	276,000	83	154	85,000	57%		
Total Girders					9,240.00	#####			24																						
					LF				Ea	1 shift = 10 Hrs																					

STANDARD WF GIRDERS	Weight (lb/ft)
WF100	815.5
WF120	858.9
WF150	918.7
WF180	979.1
WF210	1042.4
WF240	1096.1
WF270	1155.4
WF300	1242.1
WF330	1278.9



LTM 1400

Mobile Crane
Grue automotrice

Remarks referring to load chart:

- The tabulated lifting capacities do not exceed 85% of the tipping limit.
- The crane's structural strength is in accordance with EN 13018, part 3. Design and construction of the crane comply with EN 13018, part 2, and with F. E. M. regulations.
- The 85% engineering limit values take into account wind force of wind speed 20 mph.
- Lifting capacities are given in kips.
- The weight of the hook block and load hook is deducted from the lifting capacities.
- Working radii are measured from the steering centerline.
- The lifting capacities given for the telescopic boom only apply if the folding jib is taken off.
- Lifting capacities are subject to modification.

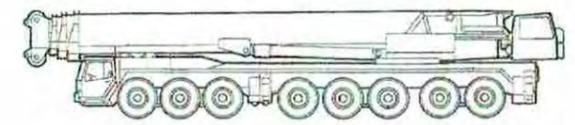
Remarques relatives aux tableaux des charges:

- Les forces de levage indiquées ne dépassent 85% de la charge de basculement.
- La norme EN 13018, partie 3, est applicable pour la construction. La construction de la grue est conforme conformément à la norme EN 13018, partie 2, et aux règles de la F. E. M.
- A 85% de la charge de basculement, il a été tenu compte d'un vent de force 6 - vitesse de vent 20 mph.
- Les forces de levage sont données en kips.
- Le poids des crochets et crochets est déduit des capacités de levage.
- Les rayons de travail sont mesurés à partir de l'axe de rotation.
- Les forces indiquées pour la flèche télescopique s'appliquent seulement si la grue est démontée.
- Les forces de levage sont modifiables sans préavis.

Lifting capacities are given in kips (1,000 lbs).



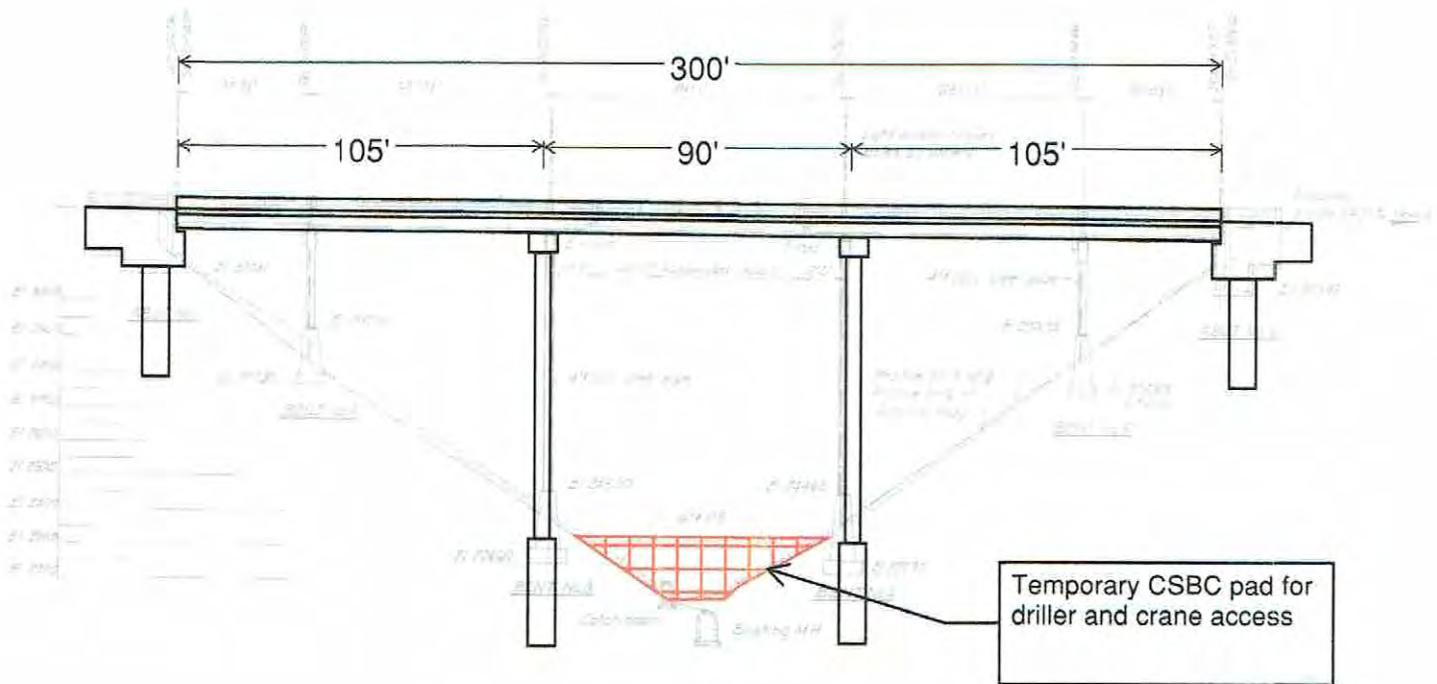
PORTLAND OFFICE: 503.283.3311
SEATTLE OFFICE: 206.364.3824
WWW.NESSCAMPBELL.COM



LIEBHERR



PORTLAND OFFICE: 503.283.3311
SEATTLE OFFICE: 206.364.3824
WWW.NESSCAMPBELL.COM



BRIDGE ELEVATION

Load chart

GRT655



Pounds

Radius in Feet	Main Boom Length in Feet									
	34.9	43.7	52.5	61.4	70.2	79	87.8	96.7	105.5	114.3
8	110,000 (68.5)	—	—	—	—	—	—	—	—	—
10	88,750 (65)	81,000 (70.5)	79,700 (74)	79,200 (77)	72,650 (79.5)	—	—	—	—	—
12	80,150 (61)	78,100 (67.5)	76,400 (72)	75,050 (75)	68,200 (77.5)	*57,800 (80)	—	—	—	—
15	69,550 (55)	68,200 (63)	66,700 (68)	65,450 (72)	60,450 (75)	51,050 (77)	42,250 (79)	*34,500 (80)	—	—
20	55,300 (43)	55,800 (55.5)	54,900 (62)	53,900 (67)	49,400 (70.5)	42,050 (73)	34,950 (75.5)	33,300 (77)	28,100 (78.5)	*25,800 (80)
25	40,400 (26)	43,300 (46.5)	43,750 (55.5)	44,100 (61.5)	41,300 (66)	35,150 (69)	29,250 (72)	28,150 (74)	27,000 (76)	24,600 (77.5)
30	—	34,750 (35)	35,200 (48)	35,500 (56)	35,250 (61.5)	29,950 (65)	24,900 (68.5)	23,950 (71)	23,150 (73)	21,800 (75)
35	—	24,950 (17)	28,150 (39.5)	28,300 (50)	28,450 (56.5)	25,950 (61)	21,500 (64.5)	20,650 (67.5)	20,000 (70)	19,400 (72)
40	—	—	22,050 (29)	22,300 (43)	22,500 (51)	22,750 (56.5)	18,750 (61)	18,050 (64.5)	17,450 (67)	16,900 (69.5)
45	—	—	—	18,050 (34.5)	18,300 (45.5)	18,500 (52)	16,550 (57)	15,900 (61)	15,350 (64)	14,900 (66.5)
50	—	—	—	14,850 (24)	15,150 (38.5)	15,300 (47)	14,700 (53)	14,100 (57.5)	13,600 (61)	13,150 (64)
55	—	—	—	—	12,700 (30.5)	12,900 (41.5)	13,000 (48.5)	12,550 (53.5)	12,100 (57.5)	11,700 (61)
60	—	—	—	—	10,700 (19)	10,900 (35)	11,050 (43.5)	11,150 (49.5)	10,850 (54)	10,500 (58)
65	—	—	—	—	—	9300 (27)	9470 (38)	9560 (45)	9570 (50.5)	9420 (54.5)
70	—	—	—	—	—	7950 (14)	8100 (32)	8220 (40.5)	8240 (46.5)	8240 (51.5)
75	—	—	—	—	—	—	6950 (23.5)	7070 (35)	7120 (42.5)	7140 (48)
80	—	—	—	—	—	—	4650 (6.5)	6070 (29)	6150 (38)	6190 (44)
85	—	—	—	—	—	—	—	5210 (20.5)	5300 (32.5)	5370 (40)
90	—	—	—	—	—	—	—	—	4550 (26)	4640 (35.5)
95	—	—	—	—	—	—	—	—	3900 (17)	3990 (30)
100	—	—	—	—	—	—	—	—	—	3410 (23.5)
105	—	—	—	—	—	—	—	—	—	2890 (13.5)
Min. boom angle for indicated length (no load)										12°
Max. boom length at 0° boom angle (no load)										105.5 ft

NOTE: () Boom angles are in degrees.

*This capacity is based on maximum boom angle.

Boom Angle	Main Boom Length in Feet									
	34.9	43.7	52.5	61.4	70.2	79	87.8	96.7	105.5	114.3
0°	17,600 (27.1)	12,700 (35.9)	9480 (44.7)	7160 (53.6)	5420 (62.4)	4060 (71.2)	2970 (80)	2070 (88.9)	1320 (97.7)	—

NOTE: () Reference radii in feet.

80100630

THIS CHART IS ONLY A GUIDE AND SHOULD NOT BE USED TO OPERATE THE CRANE.
The individual crane's load chart, operating instructions and other instructional plates must be read and understood prior to operating the crane

PCSA Class 12-1080
Refer to notes page 11

LS-248H II Lift Crane Capacities

100 FT. (30.48 m) TUBE BOOM - OPEN THROAT TOP SECTION								
Load Radius (ft)	Boom Angle (deg)	360° Rotation					Over End Blocked	Load Radius (ft)
		ABC+A CTWT (lb)	ABC CTWT (lb)	AB CTWT (lb)	A CTWT (lb)	0 CTWT (lb)		
17.2	82.0	219,200	219,200	219,200	206,100	148,500	219,200	17.2
18	81.5	216,700	216,700	216,700	185,500	133,500	216,700	18
19	81.0	213,800	213,800	213,800	164,900	118,500	213,800	19
20	80.4	210,800	210,800	210,800	148,200	106,400	210,800	20
25	77.5	193,500	193,500	157,200	97,800	69,600	193,500	25
30	74.5	159,400	144,900	117,000	72,300	51,000	159,400	30
35	71.5	129,000	115,100	92,700	56,800	39,800	133,200	35
40	68.5	106,700	95,100	76,400	46,500	32,300	114,500	40
50	62.1	78,700	70,000	56,000	33,500	22,800	88,400	50
60	55.4	61,900	54,900	43,600	25,600	17,100	70,500	60
70	48.1	50,600	44,800	35,400	20,300	13,200	58,900	70
80	39.9	42,500	37,500	29,400	16,500	10,400	49,500	80
90	29.9	36,400	32,000	24,900	13,700	8,300	42,000	90
100	14.7	31,500	27,600	21,400	11,300	6,600	35,600	100

110 FT. (33.53 m) TUBE BOOM - OPEN THROAT TOP SECTION								
Load Radius (ft)	Boom Angle (deg)	360° Rotation					Over End Blocked	Load Radius (ft)
		ABC+A CTWT (lb)	ABC CTWT (lb)	AB CTWT (lb)	A CTWT (lb)	0 CTWT (lb)		
18.6	82.0	201,100	201,100	201,100	173,100	124,500	201,100	18.6
19	81.8	200,000	200,000	200,000	165,100	118,700	200,000	19
20	81.3	197,500	197,500	197,500	148,500	106,600	197,500	20
25	78.6	185,300	185,300	157,300	97,900	69,700	185,400	25
30	75.9	158,100	144,900	117,000	72,300	51,000	158,200	30
35	73.2	128,900	115,000	92,700	56,800	39,800	132,100	35
40	70.5	106,600	95,000	76,300	46,400	32,200	113,500	40
50	64.9	78,600	69,800	55,900	33,400	22,700	87,700	50
60	59.0	61,800	54,800	43,500	25,500	17,000	69,900	60
70	52.7	50,500	44,700	35,300	20,200	13,100	58,700	70
80	45.8	42,400	37,400	29,300	16,400	10,300	49,300	80
90	38.0	36,300	31,900	24,900	13,600	8,200	42,000	90
100	28.4	31,500	27,600	21,300	11,300	6,500	36,200	100
110	14.0	27,600	24,100	18,500	9,400	5,100	31,000	110

120 FT. (36.58 m) TUBE BOOM - OPEN THROAT TOP SECTION								
Load Radius (ft)	Boom Angle (deg)	360° Rotation					Over End Blocked	Load Radius (ft)
		ABC+A CTWT (lb)	ABC CTWT (lb)	AB CTWT (lb)	A CTWT (lb)	0 CTWT (lb)		
20.0	82.0	186,200	186,200	186,200	148,900	106,900	186,200	20.0
25	79.6	173,900	173,900	157,300	97,900	69,800	173,900	25
30	77.1	156,800	144,900	117,000	72,200	51,000	156,900	30
35	74.7	128,900	115,000	92,600	56,700	39,700	131,100	35
40	72.2	106,500	94,900	76,300	46,300	32,100	112,600	40
50	67.1	78,500	69,800	55,800	33,200	22,600	86,900	50
60	61.8	61,600	54,800	43,400	25,400	16,800	69,300	60
70	56.2	50,300	44,500	35,100	20,100	13,000	58,300	70
80	50.3	42,200	37,200	29,200	16,300	10,200	49,000	80
90	43.7	36,100	31,800	24,700	13,400	8,100	41,800	90
100	36.3	31,400	27,500	21,200	11,200	6,400	36,100	100
110	27.2	27,500	24,000	18,400	9,400	5,100	31,400	110
120	13.4	24,300	21,100	16,000	7,800	3,900	27,600	120

MIN RADIUS FROM G

Appendix F. Construction Schedule

ALTERNATIVE 1 - SINGLE STAGE BRIDGE REPLACEMENT

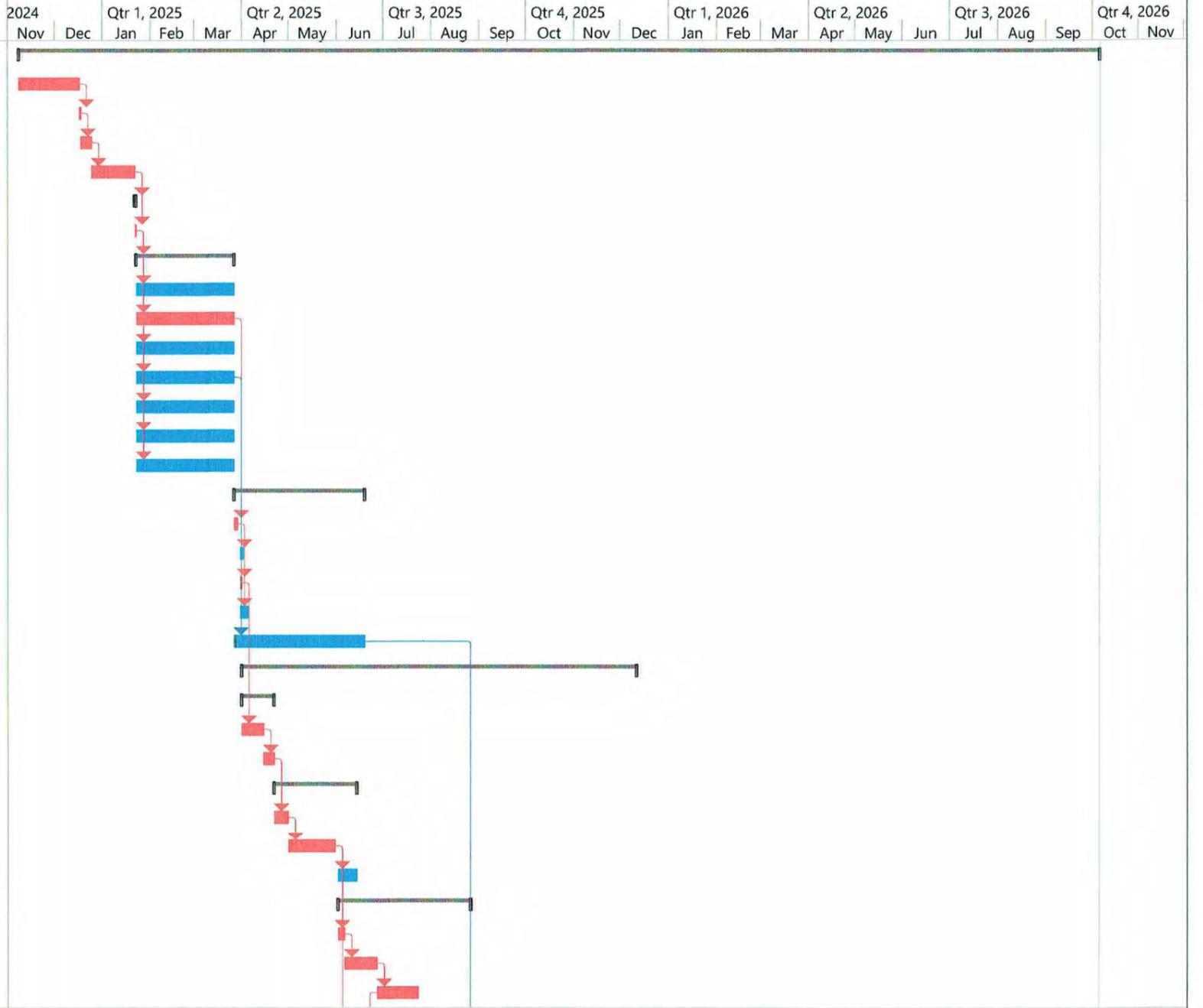
ID	Task Mode	Task Name	Duration	Start	Finish	Predecessors	R	N																															
									24	Nov	Dec	Qtr 1, 2025		Jan	Feb	Mar	Qtr 2, 2025		Apr	May	Jun	Qtr 3, 2025		Jul	Aug	Sep	Qtr 4, 2025		Oct	Nov	Dec	Qtr 1, 2026		Jan	Feb	Mar	Qtr 2, 2026		Apr
1		SW Admiral Way Bridge Single Phase	361 days	Fri 11/8/24	Fri 3/27/26																																		
2		Advertise	27 days	Fri 11/8/24	Mon 12/16/24																																		
3		Bid	1 day	Tue 12/17/24	Tue 12/17/24	2																																	
4		Award	5 days	Wed 12/18/24	Tue 12/24/24	3																																	
5		Execute Contract	20 days	Wed 12/25/24	Tue 1/21/25	4																																	
6		NTP	1 day	Wed 1/22/25	Wed 1/22/25	5																																	
7		NTP	1 day	Wed 1/22/25	Wed 1/22/25	5																																	
8		Submittals/Approvals	45 days	Thu 1/23/25	Wed 3/26/25	7																																	
9		Safety Plan	45 days	Thu 1/23/25	Wed 3/26/25	7																																	
10		TESC Plan	45 days	Thu 1/23/25	Wed 3/26/25	7																																	
11		Shaft Plan	45 days	Thu 1/23/25	Wed 3/26/25	7																																	
12		Concrete Plan	45 days	Thu 1/23/25	Wed 3/26/25	7																																	
13		Girder Submittal	45 days	Thu 1/23/25	Wed 3/26/25	7																																	
14		Demo Plan	45 days	Thu 1/23/25	Wed 3/26/25	7																																	
15		Traffic Control Plan	45 days	Thu 1/23/25	Wed 3/26/25	7																																	
16		Mobilize	60 days	Thu 3/27/25	Wed 6/18/25																																		
17		Set Up TESC	2 days	Thu 3/27/25	Fri 3/28/25	10																																	
18		Temporary Fencing	2 days	Mon 3/31/25	Tue 4/1/25	17																																	
19		Set Traffic Control	1 day	Mon 3/31/25	Mon 3/31/25	17																																	
20		Project Trailers	5 days	Mon 3/31/25	Fri 4/4/25	17																																	
21		Procure Girders	60 days	Thu 3/27/25	Wed 6/18/25	12																																	
22		Construction	228 days	Tue 4/1/25	Thu 2/12/26																																		
23		Demo Existing Bridge	25 days	Tue 4/1/25	Mon 5/5/25																																		
24		Sawcut and Remove Panels	15 days	Tue 4/1/25	Mon 4/21/25	19																																	
25		Demo Substructure	10 days	Tue 4/22/25	Mon 5/5/25	24																																	
26		Foundation	47 days	Tue 5/6/25	Wed 7/9/25																																		
27		Build Temp Drill Pad	7 days	Tue 5/6/25	Wed 5/14/25	25																																	
28		Drilled Shafts	25 days	Thu 5/15/25	Wed 6/18/25	27																																	
29		Soil Nails	15 days	Thu 6/19/25	Wed 7/9/25	28																																	
30		Substructure (Incl Cure)	63 days	Thu 6/19/25	Mon 9/15/25																																		
31		FRP Abutment Cap/Wall E&W	20 days	Thu 6/19/25	Wed 7/16/25	28																																	

Project: Admiral Way - 1 Phase R2
Date: Fri 5/10/24

Task		Inactive Task	Manual Summary Rollup		External Milestone		Manual Progress	
Split		Inactive Milestone	Manual Summary		Deadline			
Milestone		Inactive Summary	Start-only		Critical			
Summary		Manual Task	Finish-only		Critical Split			
Project Summary		Duration-only	External Tasks		Progress			

ALTERNATIVE 2 - BRIDGE REPLACEMENT WITH STAGED CONSTRUCTION

ID	Task Mode	Task Name	Duration	Start	Finish	Predecessors	RN
1		SW Admiral Way Bridge Replacement	497 days	Fri 11/8/24	Mon 10/5/26		
2		Advertise	27 days	Fri 11/8/24	Mon 12/16/24		
3		Bid	1 day	Tue 12/17/24	Tue 12/17/24	2	
4		Award	5 days	Wed 12/18/24	Tue 12/24/24	3	
5		Execute Contract	20 days	Wed 12/25/24	Tue 1/21/25	4	
6		NTP	1 day	Wed 1/22/25	Wed 1/22/25	5	
7		NTP	1 day	Wed 1/22/25	Wed 1/22/25	5	
8		Submittals/Approvals	45 days	Thu 1/23/25	Wed 3/26/25	7	
9		Safety Plan	45 days	Thu 1/23/25	Wed 3/26/25	7	
10		TESC Plan	45 days	Thu 1/23/25	Wed 3/26/25	7	
11		Shaft Plan	45 days	Thu 1/23/25	Wed 3/26/25	7	
12		Concrete Plan	45 days	Thu 1/23/25	Wed 3/26/25	7	
13		Girder Submittal	45 days	Thu 1/23/25	Wed 3/26/25	7	
14		Demo Plan	45 days	Thu 1/23/25	Wed 3/26/25	7	
15		Traffic Control Plan	45 days	Thu 1/23/25	Wed 3/26/25	7	
16		Mobilize	60 days	Thu 3/27/25	Wed 6/18/25		
17		Set Up TESC	2 days	Thu 3/27/25	Fri 3/28/25	10	
18		Temporary Fencing	2 days	Mon 3/31/25	Tue 4/1/25	17	
19		Set Traffic Control	1 day	Mon 3/31/25	Mon 3/31/25	17	
20		Project Trailers	5 days	Mon 3/31/25	Fri 4/4/25	17	
21		Procure Girders	60 days	Thu 3/27/25	Wed 6/18/25	12	
22		Phase 1	183 days	Tue 4/1/25	Thu 12/11/25		
23		Demo North Bridge	15 days	Tue 4/1/25	Mon 4/21/25		
24		Sawcut and Remove Panels	10 days	Tue 4/1/25	Mon 4/14/25	19	
25		Demo Substructure	5 days	Tue 4/15/25	Mon 4/21/25	24	
26		Foundation	39 days	Tue 4/22/25	Fri 6/13/25		
27		Build Temp Drill Pad	7 days	Tue 4/22/25	Wed 4/30/25	25	
28		Drilled Shafts	22 days	Thu 5/1/25	Fri 5/30/25	27	
29		Soil Nails	10 days	Mon 6/2/25	Fri 6/13/25	28	
30		Substructure (Incl Cure) Phase	62 days	Mon 6/2/25	Tue 8/26/25		
31		F&I Temp Shoring at Abutm	4 days	Mon 6/2/25	Thu 6/5/25	28	
32		FRP Abutment Cap/Wall E&I	15 days	Fri 6/6/25	Thu 6/26/25	31	
33		Pier 2 and 3 Columns	19 days	Fri 6/27/25	Wed 7/23/25	32	

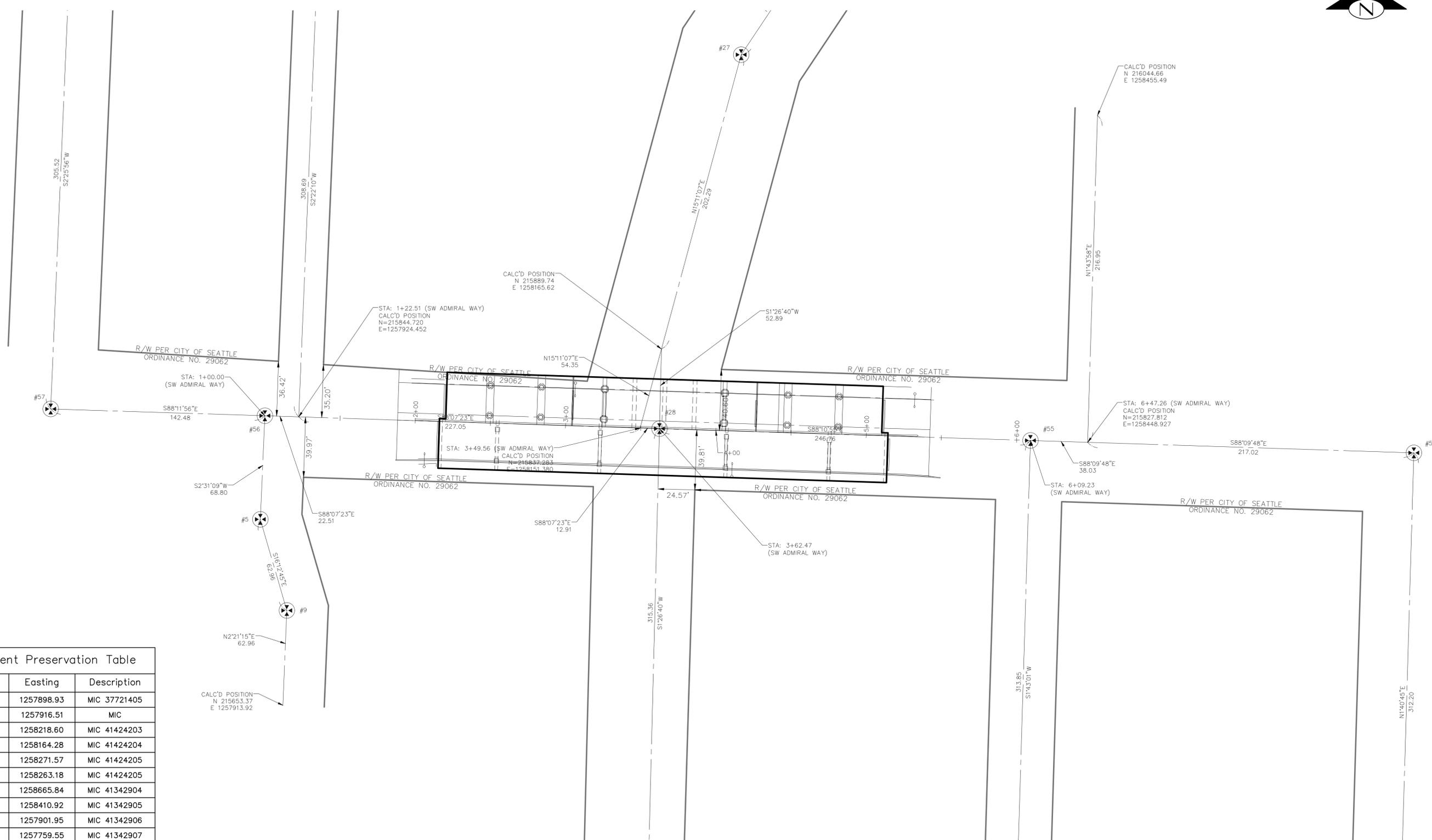


Project: Admiral Way - 2 Phase R2 Date: Fri 5/10/24	Task		Inactive Task	Manual Summary Rollup		External Milestone		Manual Progress	
	Split		Inactive Milestone	Manual Summary		Deadline			
	Milestone		Inactive Summary	Start-only		Critical			
	Summary		Manual Task	Finish-only		Critical Split			
	Project Summary		Duration-only	External Tasks		Progress			

Appendix G. Conceptual Bridge Replacement Plans



VAULT SERIAL NO.	DATE	MARK	NATURE	MADE	CHK'D	REV'D
38503						



Point #	Northing	Easting	Description
5	215776.73	1257898.93	MIC 37721405
9	215716.27	1257916.51	MIC
27	216084.97	1258218.60	MIC 41424203
28	215836.86	1258164.28	MIC 41424204
29	216164.22	1258271.57	MIC 41424205
30	216164.33	1258263.18	MIC 41424205
54	215820.86	1258665.84	MIC 41342904
55	215829.03	1258410.92	MIC 41342905
56	215845.46	1257901.95	MIC 41342906
57	215849.94	1257759.55	MIC 41342907
58	216155.18	1257772.51	BC 41342908
59	216153.15	1257937.21	MIC 41342909
105	215508.79	1258656.69	MIC 40974706
106	215515.32	1258401.51	MIC 40974707

FIGURE 1
RIGHT OF WAY PLAN

	APPROVED FOR ADVERTISING DEPARTMENT OF FINANCE & ADMINISTRATIVE SERVICES SEATTLE, WASHINGTON 20 BY: PURCHASING AND CONTRACTING	INITIALS AND DATE DESIGNED AC CHECKED MJ DRAWN TH CHECKED AC	INITIALS AND DATE REVIEWED: DES. CONST. SDOT PROJ. MGR. RECEIVED REVISED AS BUILT	 ORDINANCE NO. PW NO. SCALE: NTS	SW ADMIRAL WAY BRIDGES REPLACEMENT STUDY	JOB PC CO VPI # SHEET 1 OF 6
	ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE CITY OF SEATTLE STANDARD PLANS AND SPECIFICATIONS AND OTHER DOCUMENTS CALLED FOR IN SECTION 0-02.3 OF THE PROJECT MANUAL.					

C:\pwworkdir\den003\ch2\mill_thegim\4163271\FIGURE 1.dwg
 May-31-24 9:53am

VAULT SERIAL NO.	DATE	MARK	NATURE	MADE	CHK'D	REV'D

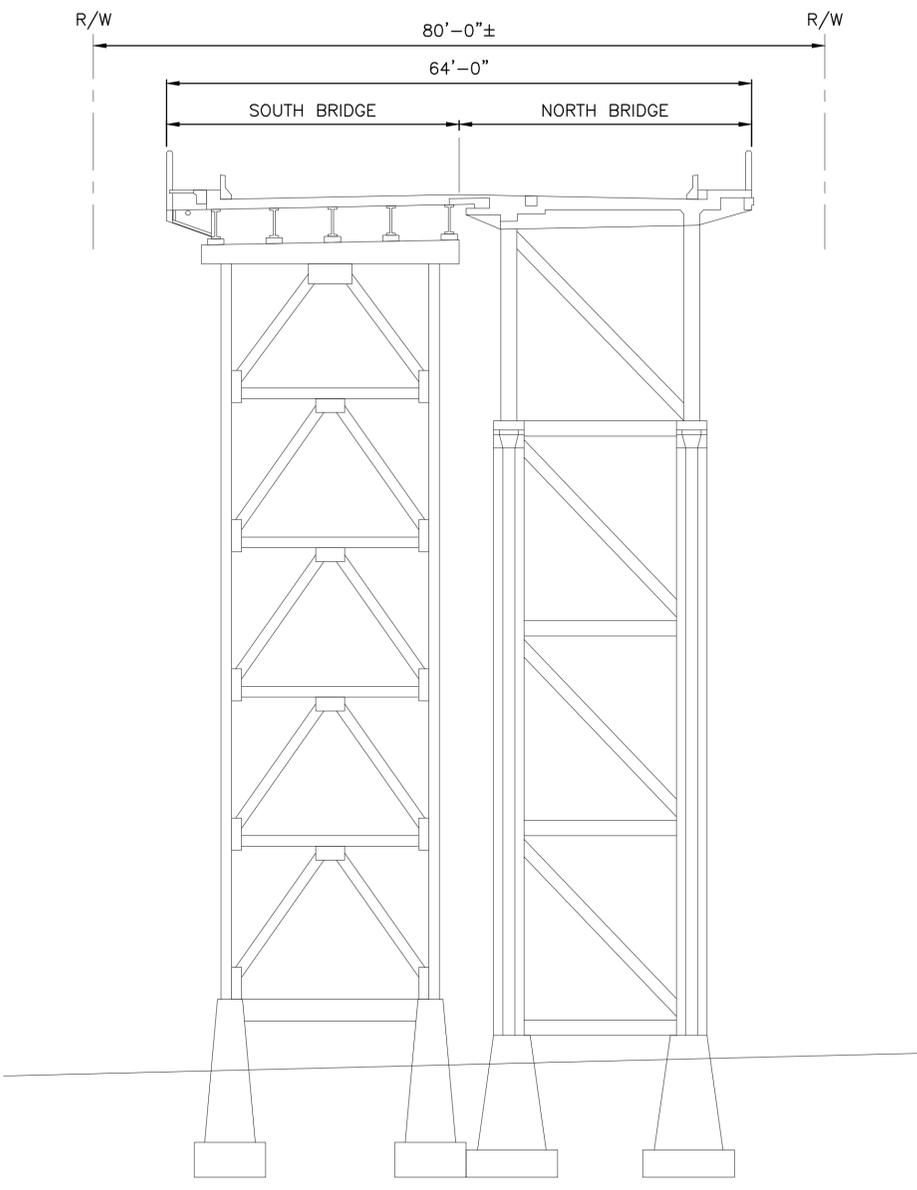
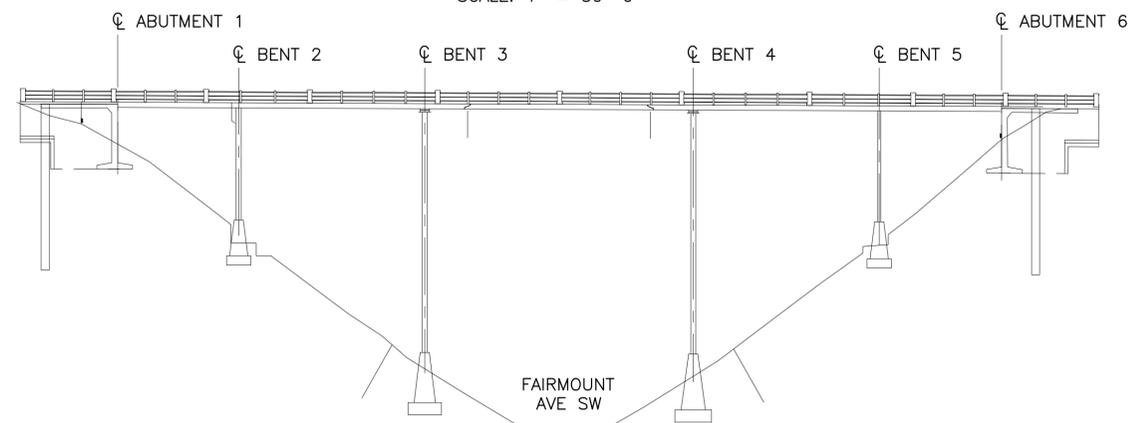
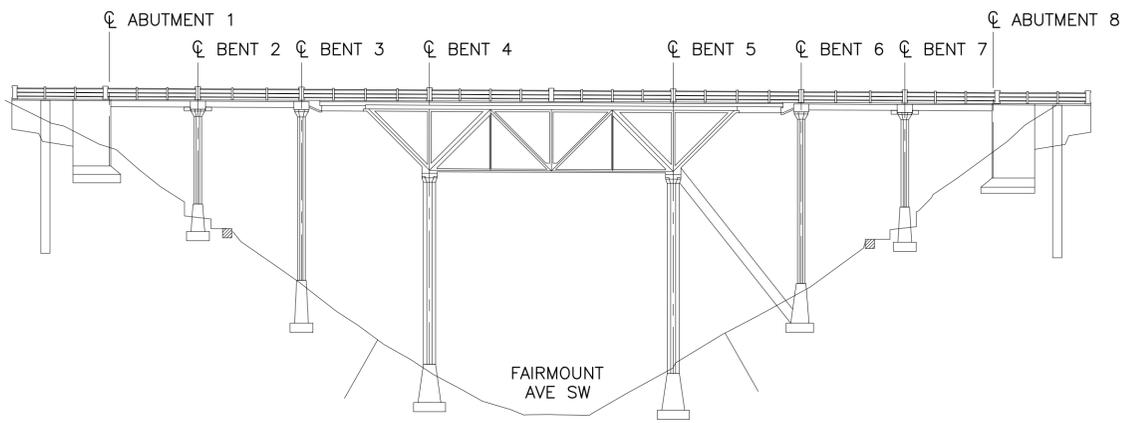
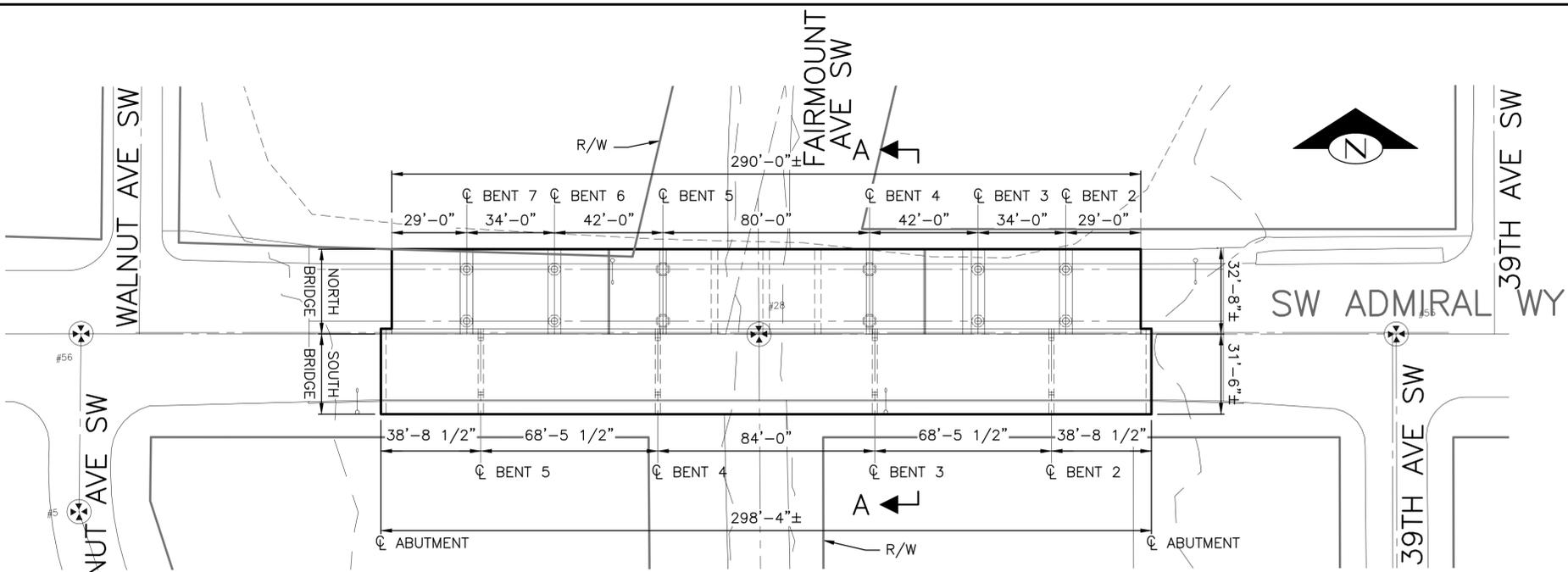


FIGURE 2
EXISTING BRIDGES



APPROVED FOR ADVERTISING		INITIALS AND DATE		INITIALS AND DATE	
DEPARTMENT OF FINANCE & ADMINISTRATIVE SERVICES SEATTLE, WASHINGTON 20		DESIGNED AC	REVIEWED: DES.	CONST.	
BY:		CHECKED MJ	SDOT	PROJ. MGR.	
PURCHASING AND CONTRACTING		DRAWN TH	RECEIVED		
		CHECKED AC	REVISED AS BUILT		
ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE CITY OF SEATTLE STANDARD PLANS AND SPECIFICATIONS AND OTHER DOCUMENTS CALLED FOR IN SECTION 0-02.3 OF THE PROJECT MANUAL.					

Seattle
Department of
Transportation

ORDINANCE NO. PW NO.

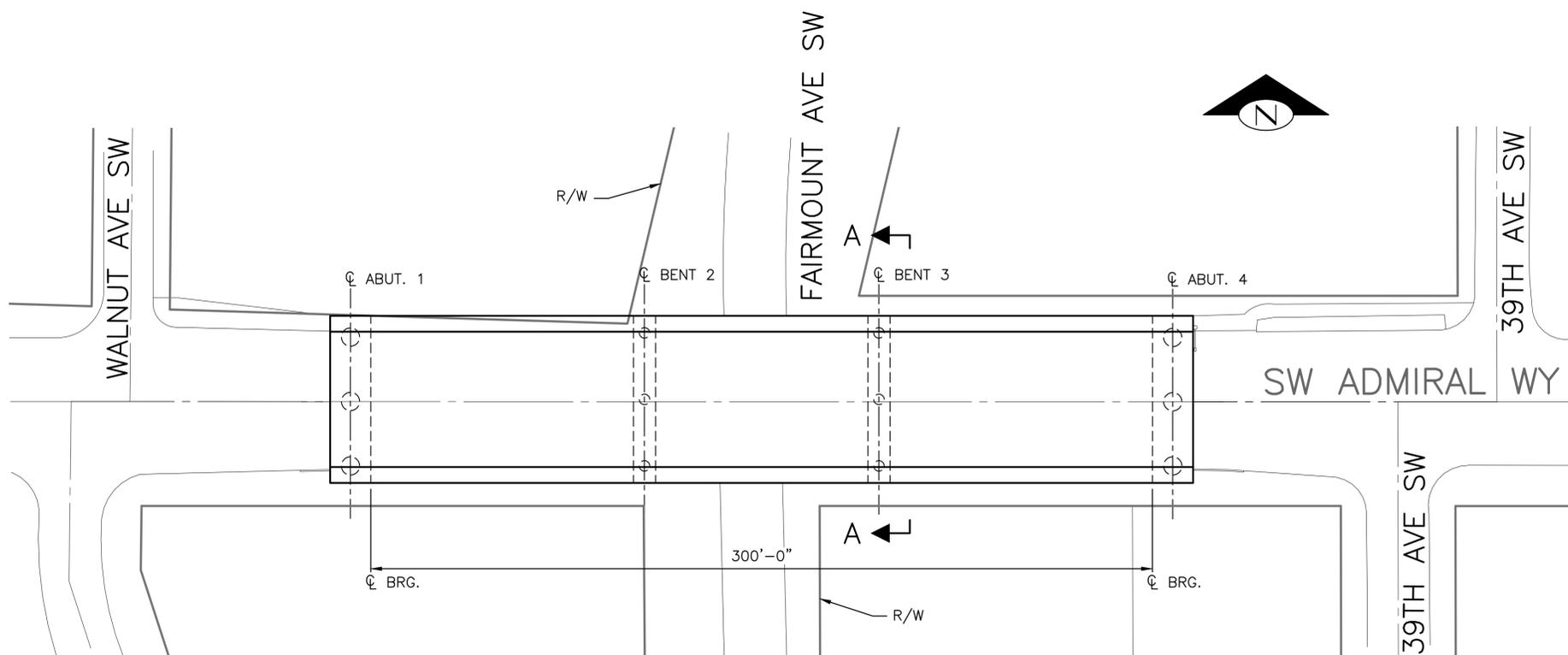
SCALE: AS NOTED

**SW ADMIRAL WAY BRIDGES
REPLACEMENT STUDY**

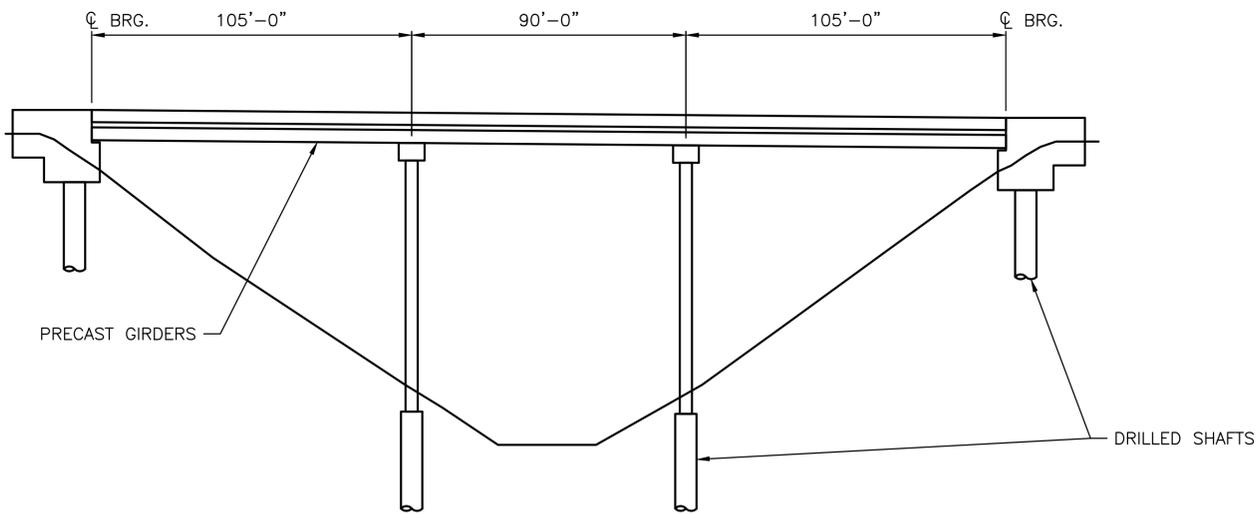
JOB	PC
CO	
VPI #	
SHEET	2 OF 6

C:\pwworkdir\den003\ch2\mill_theagin\163271\FIGURE 2.dwg
May-31-24 9:54am

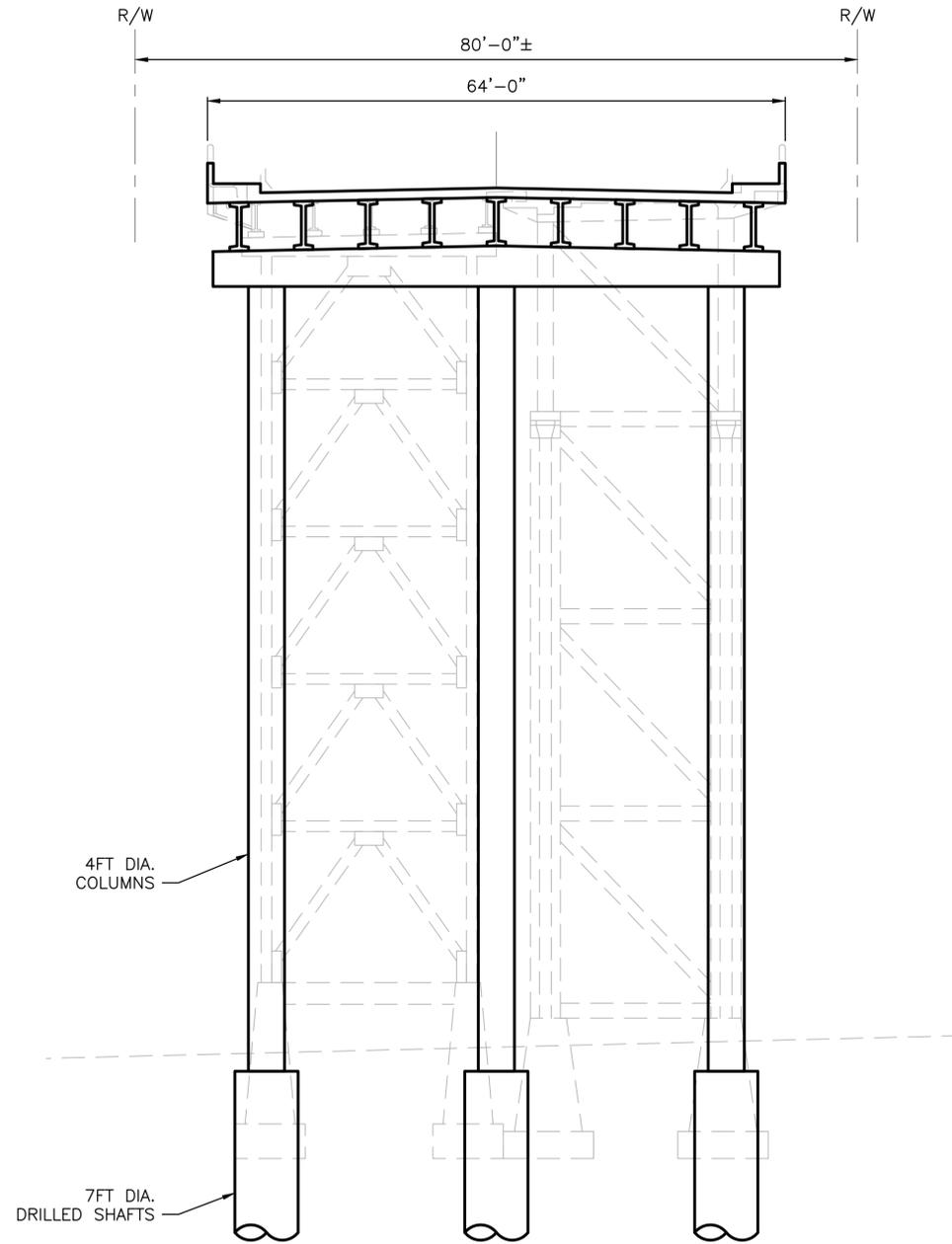
VAULT SERIAL NO.	DATE	MARK	NATURE	MADE	CHK'D	REV'D



PLAN
SCALE: 1" = 30'-0"



ELEVATION
SCALE: 1" = 30'-0"



SECTION A-A
SCALE: 1" = 10'-0"

FIGURE 3
ALTERNATIVE 1



APPROVED FOR ADVERTISING
DEPARTMENT OF FINANCE & ADMINISTRATIVE SERVICES
SEATTLE, WASHINGTON 20
BY:
PURCHASING AND CONTRACTING

INITIALS AND DATE		INITIALS AND DATE	
DESIGNED AC	CHECKED MJ	REVIEWED: DES. SDOT	CONST. PROJ. MGR.
DRAWN TH	CHECKED AC	RECEIVED	REVISED AS BUILT
ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE CITY OF SEATTLE STANDARD PLANS AND SPECIFICATIONS AND OTHER DOCUMENTS CALLED FOR IN SECTION 0-02.3 OF THE PROJECT MANUAL.			

Seattle
Department of
Transportation
ORDINANCE NO. PW NO.
SCALE: AS NOTED

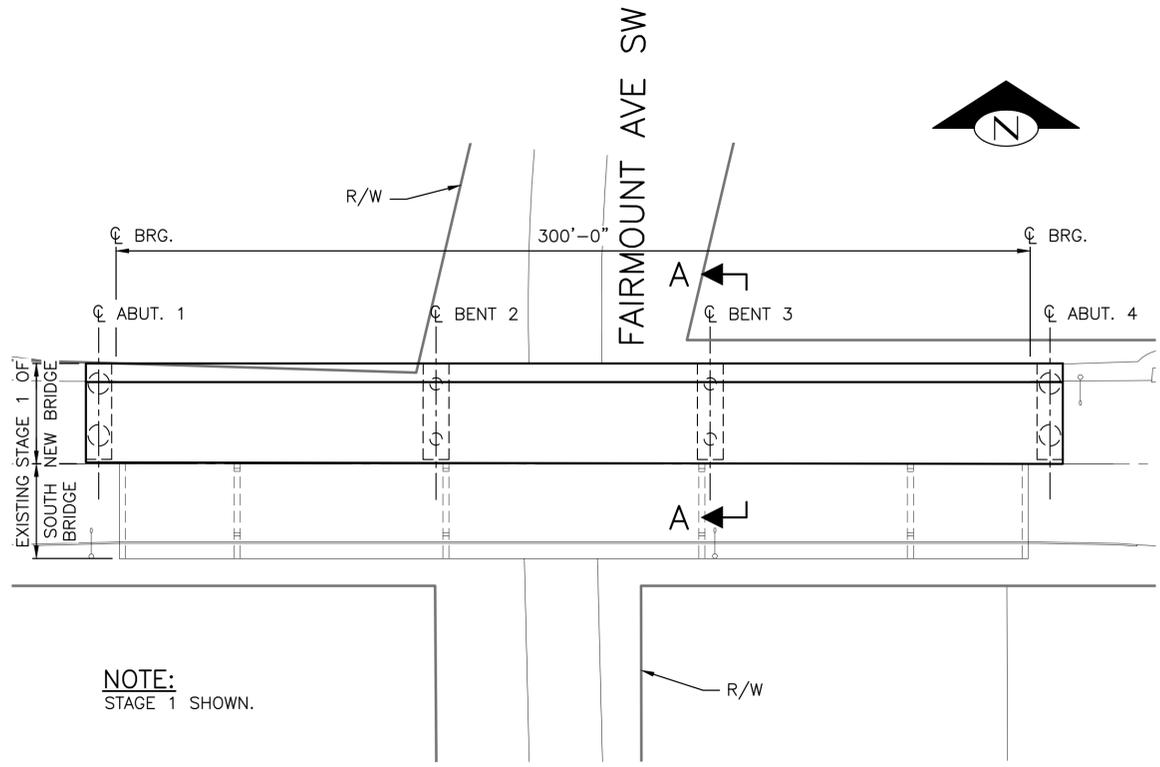
SW ADMIRAL WAY BRIDGES
REPLACEMENT STUDY

JOB	PC
CO	
VPI #	
SHEET	3 OF 6

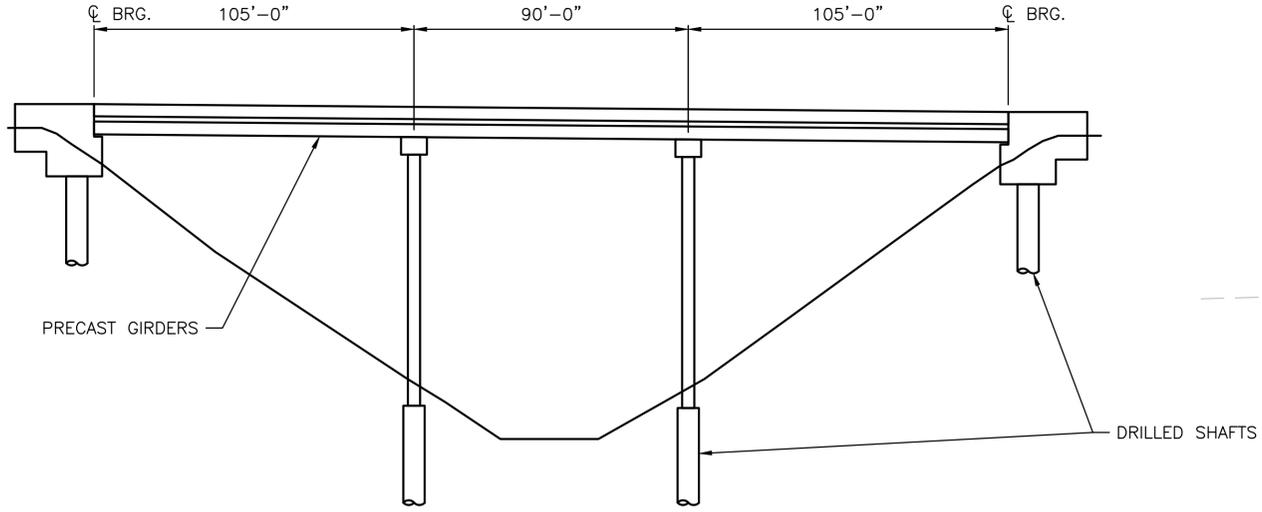
C:\pwworkdir\den003\ch2\mhill_theed\p1\163271\FIGURE 3.dwg
May-31-24 9:54am

C:\pwworkdir\den003\ch2\mhill_thegip\163271\FIGURE_4.dwg
 May-31-24 9:55am

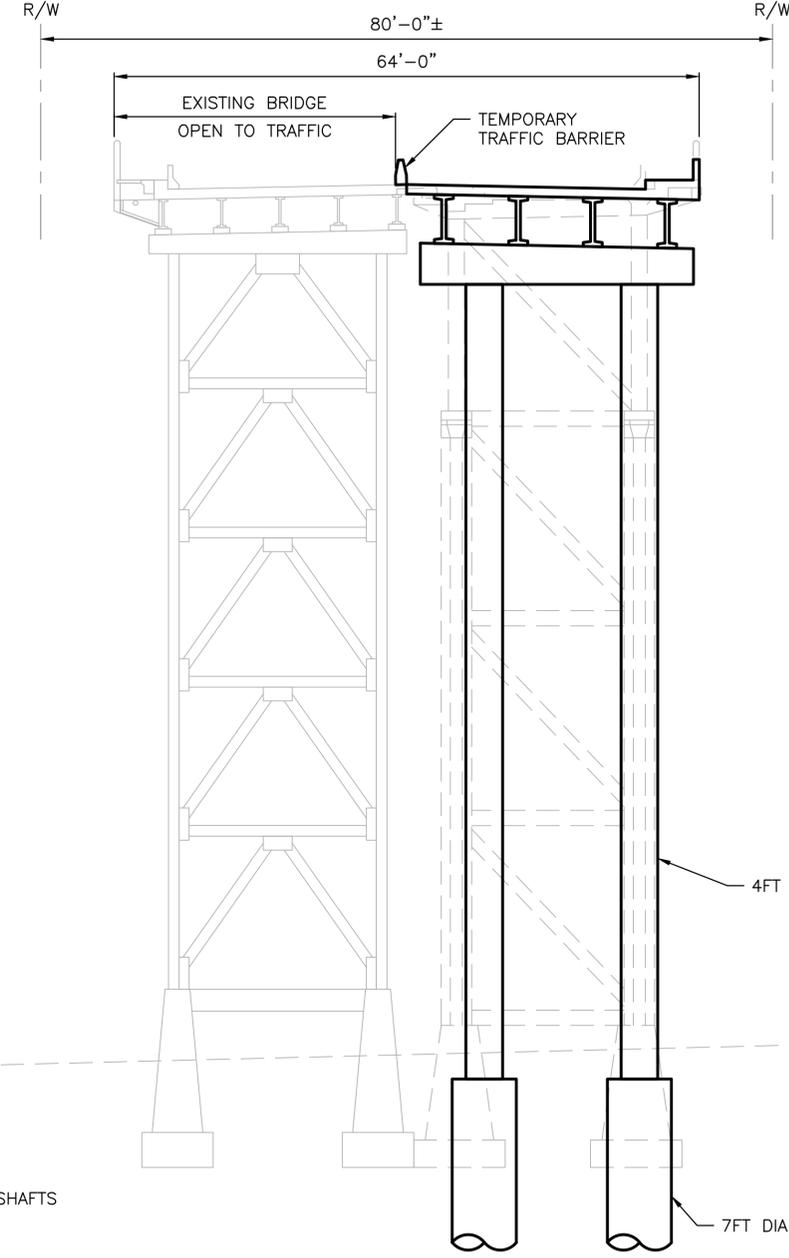
VAULT SERIAL NO.	DATE	MARK	NATURE	MADE	CHK'D	REV'D



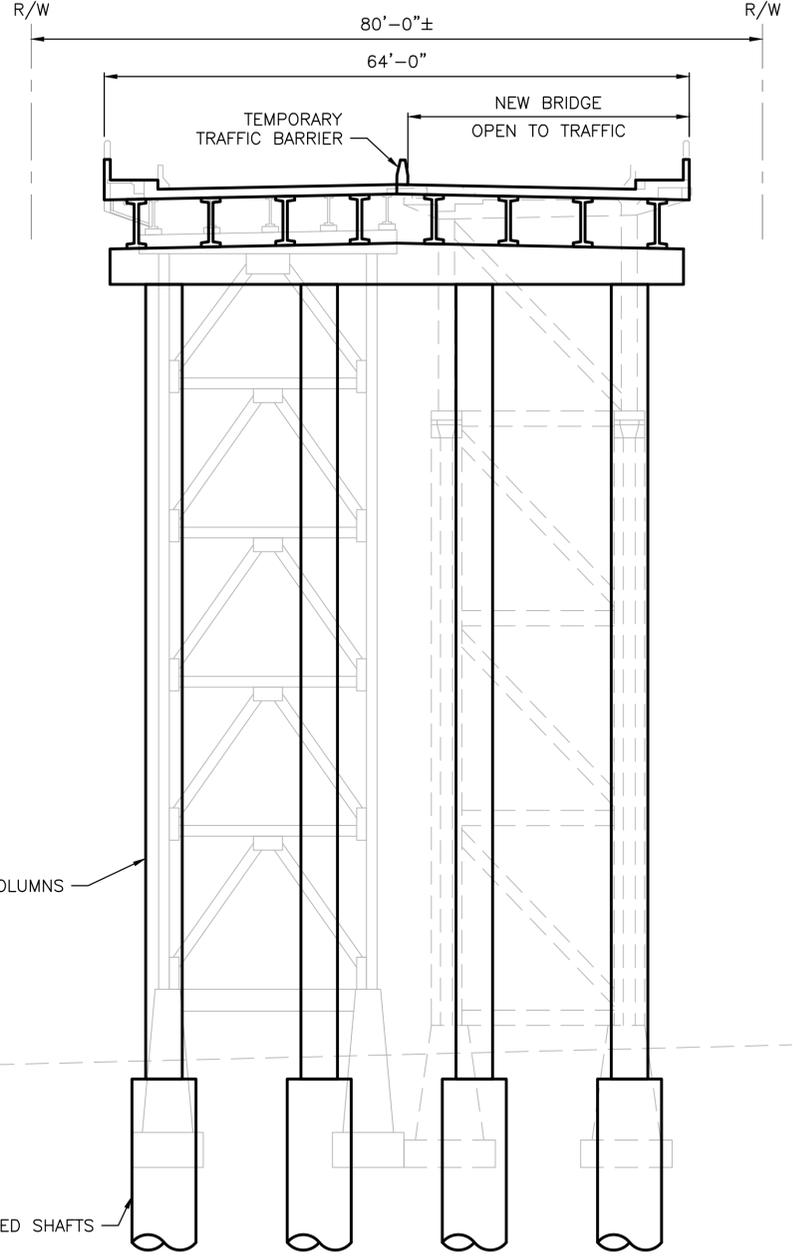
PLAN
SCALE: 1" = 30'-0"



ELEVATION
SCALE: 1" = 30'-0"



SECTION A-A
STAGE 1
SCALE: 1" = 10'-0"



SECTION A-A
STAGE 2
SCALE: 1" = 10'-0"

FIGURE 4
ALTERNATIVE 2



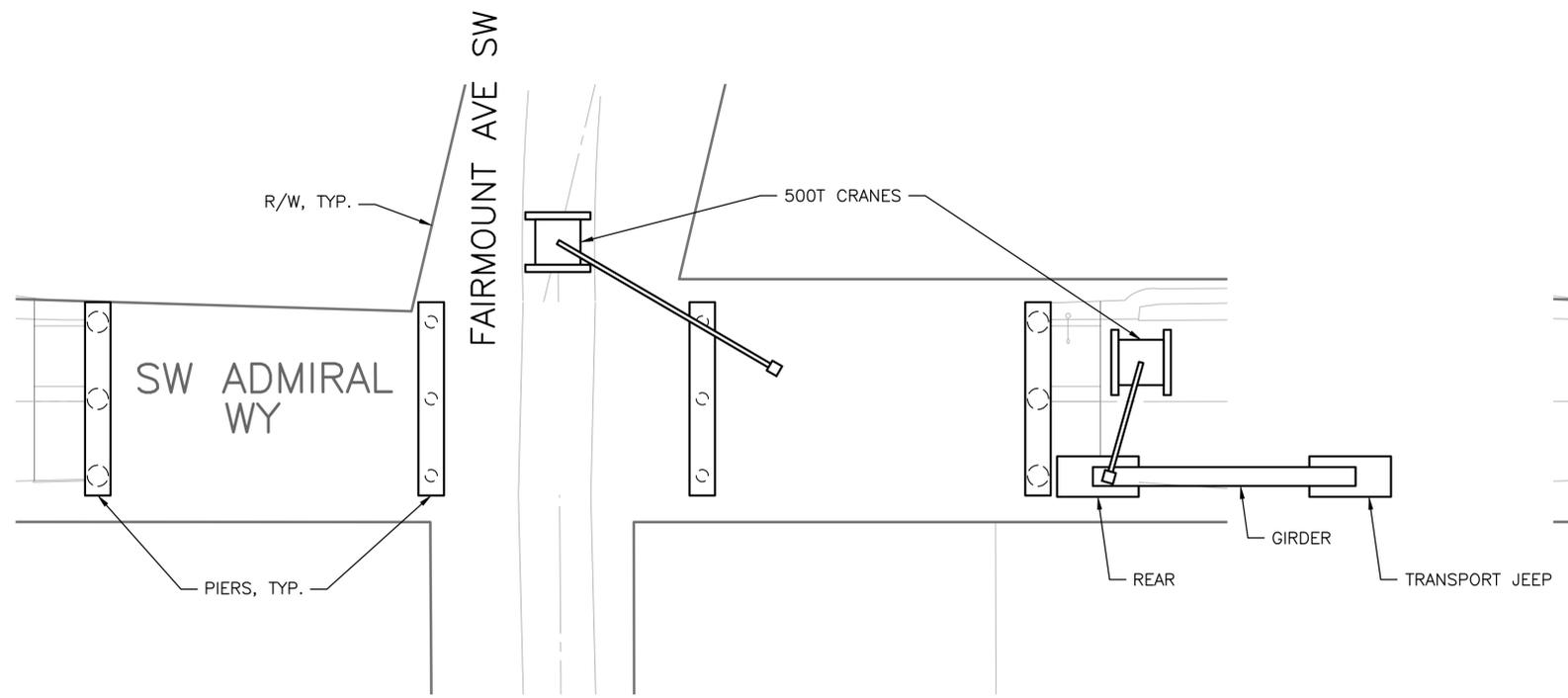
APPROVED FOR ADVERTISING
 DEPARTMENT OF FINANCE & ADMINISTRATIVE SERVICES
 SEATTLE, WASHINGTON 20
 BY:
 PURCHASING AND CONTRACTING

INITIALS AND DATE		INITIALS AND DATE	
DESIGNED AC	CHECKED MJ	REVIEWED: DES. SDOT	CONST. PROJ. MGR.
DRAWN TH	CHECKED AC	RECEIVED	REVISED AS BUILT
ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE CITY OF SEATTLE STANDARD PLANS AND SPECIFICATIONS AND OTHER DOCUMENTS CALLED FOR IN SECTION 0-02.3 OF THE PROJECT MANUAL.			

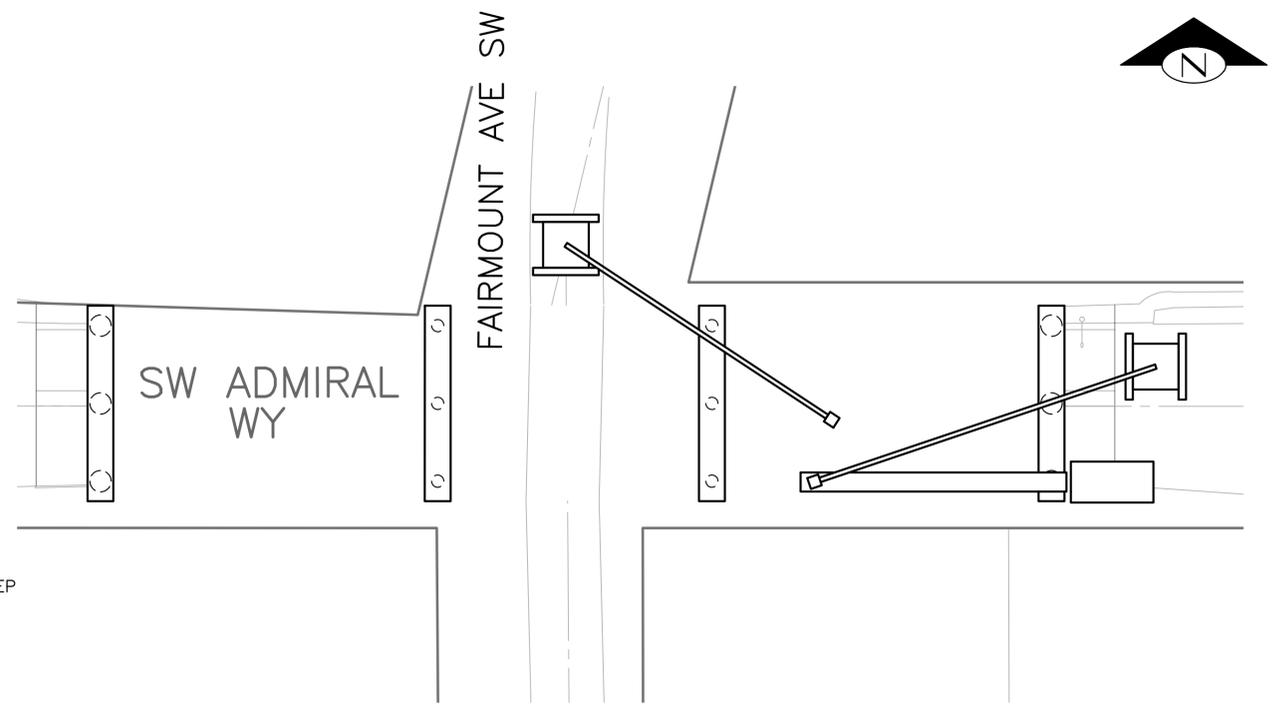
Seattle
 Department of
 Transportation
 ORDINANCE NO. PW NO.
 SCALE: AS NOTED

SW ADMIRAL WAY BRIDGES
 REPLACEMENT STUDY

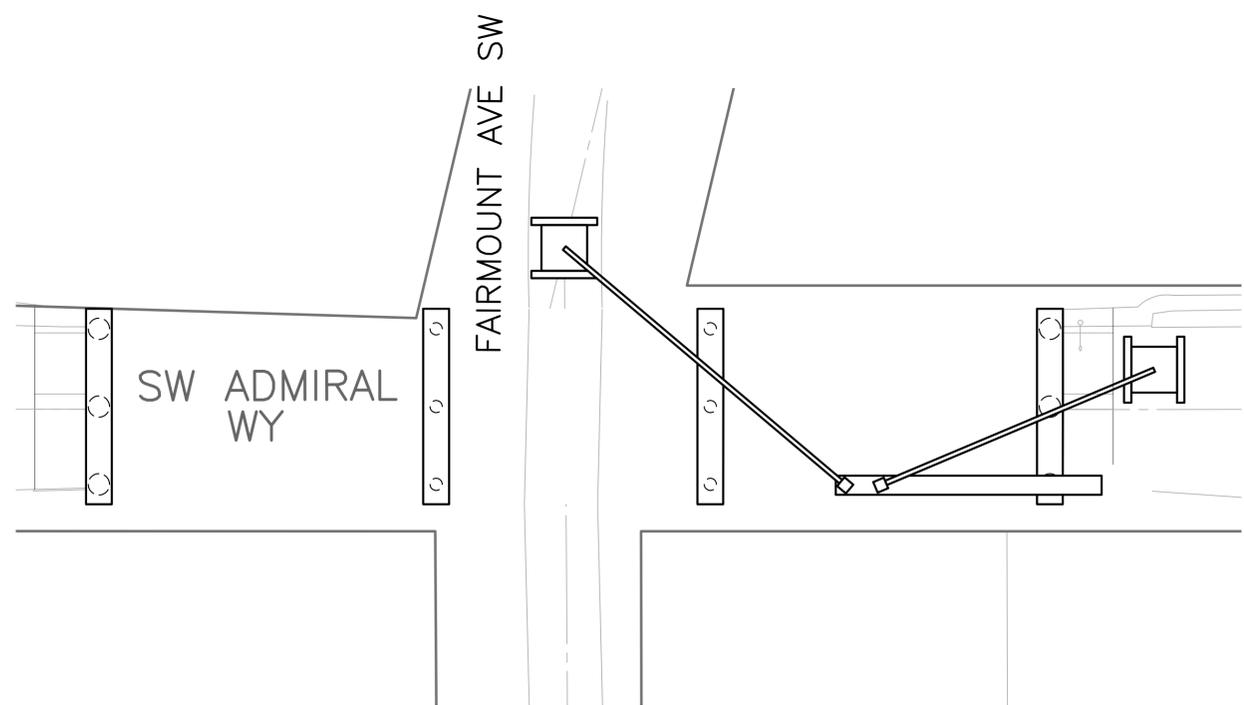
JOB	PC
CO	
VPI #	
SHEET	4 OF 6



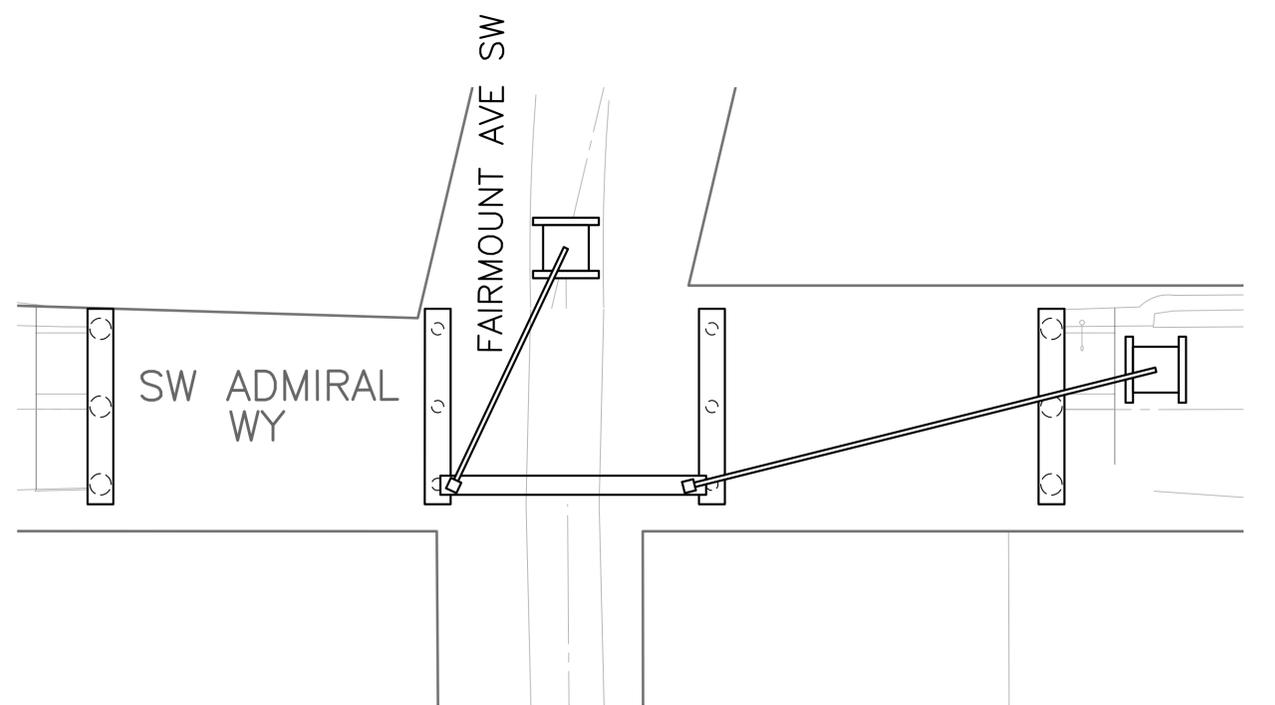
STAGE 1
CRANES AND GIRDER STAGED



STAGE 2
ABUTMENT CRANE LIFTS AND SWINGS END OF GIRDER WHILE TRANSPORT BACKS UP



STAGE 3
ABUTMENT CRANE HANDS OFF TO VALLEY CRANE AND THEN GRABS THE OTHER END OF GIRDER



STAGE 4
BOTH CRANES SWING GIRDER INTO FINAL LOCATION

FIGURE 5
ALTERNATIVE 1 – GIRDER SET SEQUENCE

VAULT SERIAL NO.	DATE	MARK	NATURE	REVISIONS	MADE	CHK'D	REV'D

C:\pwworkdir\den003\ch2mhill_thegjm\4163271\FIGURE 5.dwg
May-31-24 9:57am



APPROVED FOR ADVERTISING		INITIALS AND DATE		INITIALS AND DATE	
DEPARTMENT OF FINANCE & ADMINISTRATIVE SERVICES SEATTLE, WASHINGTON 20		DESIGNED AC	CHECKED MJ	REVIEWED: DES. SDOT	CONST. PROJ. MGR.
BY:		DRAWN TH	CHECKED AC	RECEIVED	REVISED AS BUILT
PURCHASING AND CONTRACTING		ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE CITY OF SEATTLE STANDARD PLANS AND SPECIFICATIONS AND OTHER DOCUMENTS CALLED FOR IN SECTION 0-02.3 OF THE PROJECT MANUAL.			

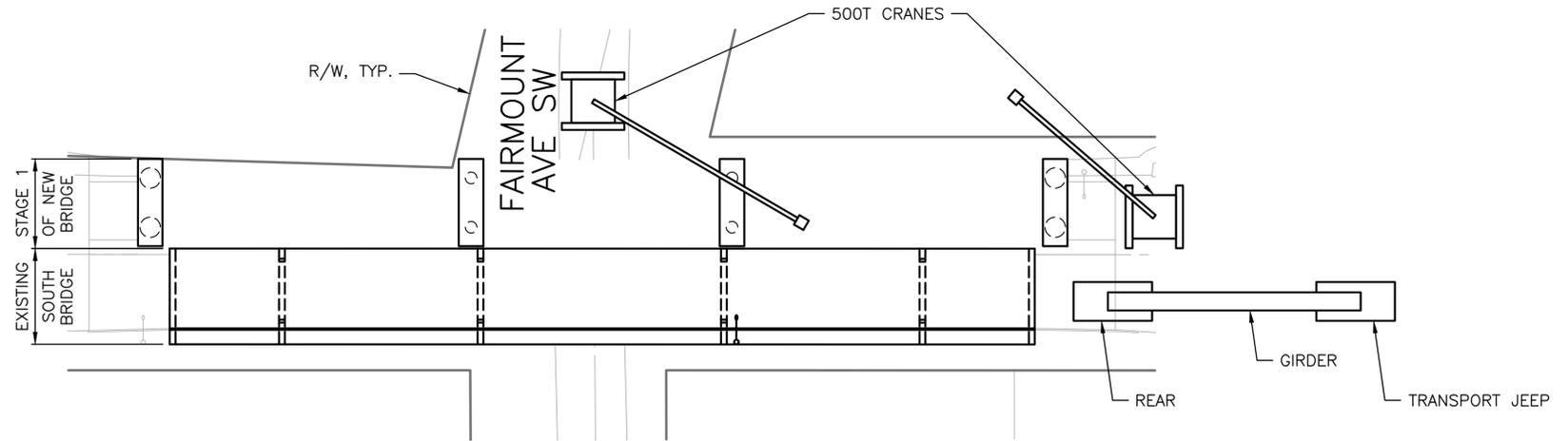
Seattle
Department of
Transportation

ORDINANCE NO. PW NO.

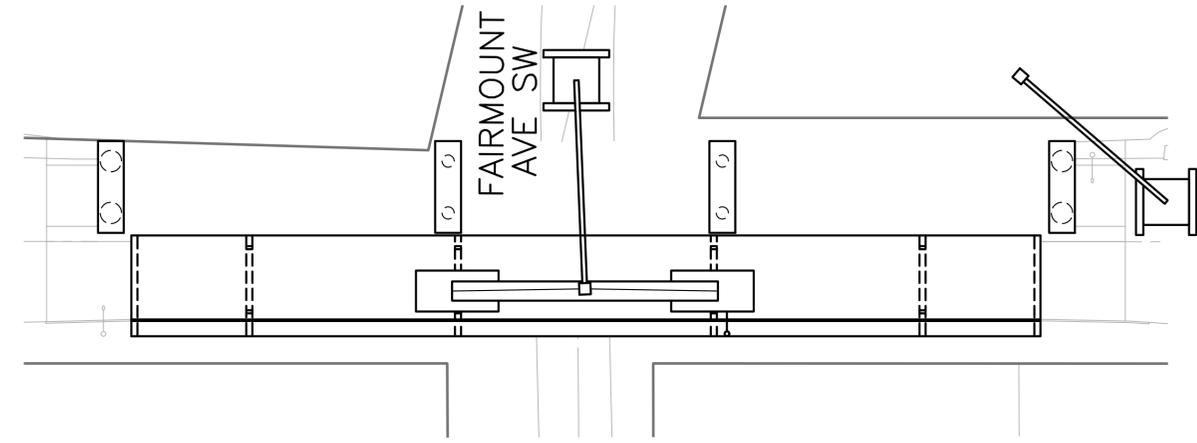
SCALE: NONE

SW ADMIRAL WAY BRIDGES
REPLACEMENT STUDY

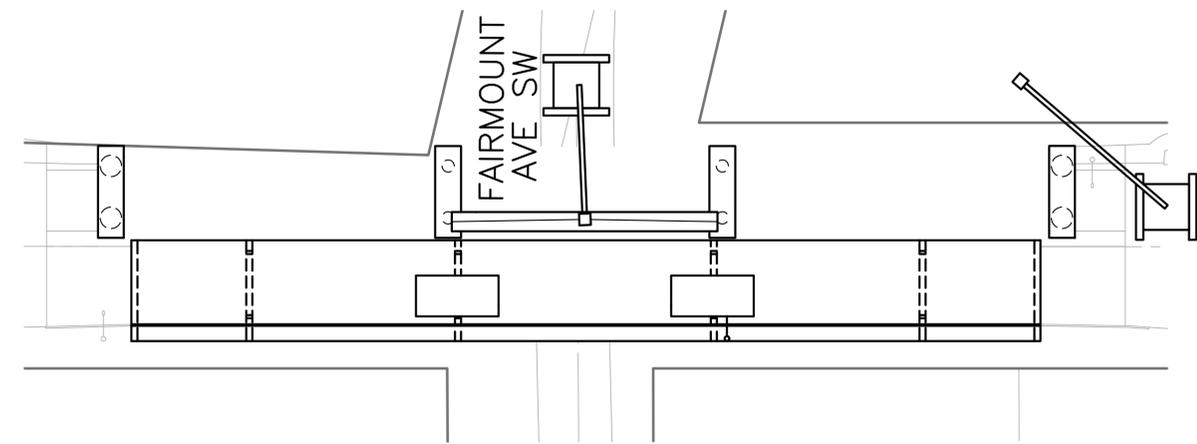
JOB	PC
CO	
VPI #	
SHEET	5 OF 6



STAGE 1
GIRDER ARRIVES TO SITE ON TRANSPORTER



STAGE 2
TRANSPORTER MOVES ONTO EXISTING STRUCTURE



STAGE 3
VALLEY CRANE PICKS UP AND PLACES GIRDER

FIGURE 6
ALTERNATIVE 2 – GIRDER SET SEQUENCE

VAULT SERIAL NO.	DATE	MARK	NATURE	MADE	CHK'D	REV'D

C:\pwworkdir\den003\ch2\mhill_theed\p14163271\FIGURE 6.dwg
May-31-24 9:57am



APPROVED FOR ADVERTISING
DEPARTMENT OF FINANCE & ADMINISTRATIVE SERVICES
SEATTLE, WASHINGTON 20
BY:
PURCHASING AND CONTRACTING

INITIALS AND DATE		INITIALS AND DATE	
DESIGNED AC		REVIEWED: DES.	CONST.
CHECKED MJ		SDOT	PROJ. MGR.
DRAWN TH		RECEIVED	
CHECKED AC		REVISED AS BUILT	
ALL WORK SHALL BE DONE IN ACCORDANCE WITH THE CITY OF SEATTLE STANDARD PLANS AND SPECIFICATIONS AND OTHER DOCUMENTS CALLED FOR IN SECTION 0-02.3 OF THE PROJECT MANUAL.			

Seattle
Department of
Transportation
ORDINANCE NO. PW NO.
SCALE: NONE

SW ADMIRAL WAY BRIDGES
REPLACEMENT STUDY

JOB	PC
CO	
VPI #	
SHEET	6 OF 6

Appendix H. Right-of-Way Assessment on SW Admiral Way

Admiral Way Bridge Replacement Study

Overview of Right-of-Way Assessment on SW Admiral Way

As SDOT began the process of evaluating potential replacement options for the SW Admiral Way Bridge, SPU Survey was tasked with performing a topographic survey of the existing conditions along the project corridor – this included the edges of the bridge, buildings/homes, roadways, and other relevant features. SPU Survey was also tasked with establishing the location of the existing rights-of-way for SW Admiral Way along this corridor, as any replacement options would likely require an increase of the footprint for the new bridge structure.

During the topographic survey, SPU crews began by locating any street monumentation and/or property corners they observed for this area. While a number of right-of-way monuments were found in the field, those along SW Admiral Way were apparently set as part of a “monument line” along SW Admiral Way, and while they are controlling the location/alignment for the various cross-streets, they do not show any specific offsets/ties to the original block lines/corners for the plat along the northerly margins for SW Admiral Way.

The issues with accurately determining the existing right-of-way revolve around the fact that there is no “centerline” for SW Admiral Way, and that the areas dedicated for the right-of-way along SW Admiral Way are each described through a series of acquisitions detailed in City of Seattle Ordinance No. 29062, approved on 3/12/1910. While it appears based on review of the available records that there is room to the south of the existing bridge edge for the “current conditions”, it would depend on how much additional room might be needed to the south for any new construction, to determine if any additional right-of-way might be needed along the southerly margins for SW Admiral Way. This is not the case however along the north side of the bridge. As will be described below, existing records for the establishment of the current right-of-way, especially along the north side of SW Admiral Way are very vague as to any location of the existing right-of-way, if they show it at all. This lack of information, combined with what appears to be errors on the original plat, makes it nearly impossible to state definitively the location of the northerly margins for the roadway – this is especially the case for the northeast quadrant of SW Admiral Way & Fairmount Ave SW.

The right-of-way dedications described in Ordinance No. 29062 along the northerly margin of SW Admiral Way each began at the SW/SE corners of the respective blocks for the original plat (Blocks 91-93, from the Replat of West Seattle Land and Improvement Company’s 3rd Plat, recorded in Vol 9 of Plats, Page 58, on 8-16-1900). In reviewing the original survey field notes, along with other recorded surveys and the information in the City Ordinance, it was a fairly straight-forward process to accurately calculate the locations for the plat blocks along the south

side of SW Admiral Way, but that was not the case for the blocks along the north side of SW Admiral Way.

Based on a review of the original fieldnotes, details shown on several of the subsequent surveys and the found street “monumentation”, the original location for Blocks 91-93, Replat of West Seattle Land and Improvement Company’s 3rd Plat, were calculated and the results fit well with all relevant documentation for Blocks 92-93, but for Block 91 it was clear it was not much more than a “guess”.

Some general notes regarding this survey effort:

1. All mons shown in the CAD file were visited during the period of January – April 2022.
2. History of Plat recordings in project area, pertinent to establishing the original block locations for the north side of SW Admiral Way:
 - A. Plat of West Seattle 5 Acre Tracts – Book 2 of Plats, Pg 51, recorded 9/26/1885.
 - B. Plat of Occident Heights Addition – Book 3 of Plats, Pg 18, recorded 12/5/1888. This Plat covers “all of Lot No. 5 of the Plat of West Seattle 5 Acre Tracts”.
 - C. 3rd Plat of the West Seattle Land and Improvement Company of West Seattle, recorded in Vol 5 of Plats, Page 10, on 2/25/1890. The area in question encompasses Blocks 72 & 79.
 - D. Replat of the West Seattle Land and Improvement Company’s Third Plat, recorded in Vol 9 of Plats, Page 58, on 8-16-1900. The area in question is now encompassed within Blocks 91-93, with the width of lots in Block 93 now shown as 110’ wide (The replat changed the configuration and Block numbers – Block 93 was originally shown as Block 79, with 120’ wide lots on original plat).
3. In reviewing the original plat information, no relationship was noted between the “currently” monumented line for Admiral Way, and the south line of the Blocks for the Plat to the north (D). Since the current R/W for SW Admiral Way was established by City of Seattle Ordinance No. 29062 (Approved on 3/12/1910), and the notations for creating the new Admiral Way R/W along Blocks 91-93 started and ended at the southwest/southeast corners of the respective Blocks, it was required to build the original plat blocks first (D), to even begin these calculations.
4. SPU survey found several monuments along the “monument line” for Admiral Way – SPU #'s 57, 56, 28, 55 & 54. The distances between 57-56, and 56-28, matched very well with all historical records – these were held and the “monument line” for SW Admiral Way was created.
5. The intersection of the Centerlines for Walnut Ave and Admiral Way was held along this line, at 22.51’ east of Monument # 56, per record. The CL for 41st Ave SW was held as the line between Mons 57-58, as shown.
6. As one of the earliest of the plats mentioned above was that for Occident Heights (B), it was decided to first calculate the location for the south line of that plat, and hold this position, as it “should be” the same as the north line for Blocks 91-93 of (D), which was

plated at a later date. Two mons along the CL for SW Prince St were located by SPU Survey (#200 & #201), the distance between these two was measured at 259.86 (survey filed under Recording Number 20041026900002, BK 178/PG 120, measured at 259.85', survey filed under Recording Number 8903249001, BK 65/PG 84, measured at 259.50', and survey filed under Recording Number 20120217900002, BK 286/PG 58, measured at 259.90), Original Plat distance = 260'.

7. To create the locations for the south line of Block 93, the original sidelines for the block were held as established by holding the found monumentation, and using the information contained in the original City of Seattle survey field notes for the locations of these streets. The original location for the west line of the block was held (prior to road vacation as detailed in VO 57314), and the current right-of-way was then established using the calls described in City of Seattle Ordinance No. 29062.
8. The next step was to create the easterly line of Block 92 (D), which is also the westerly margin for Fairmount Ave SW. To determine the CL for Fairmount Ave SW, SPU held the position at found mon # 27, then struck a line from Mon #27 to a point on the established "monument line" for Admiral Way, 12.91' westerly (record/fieldnotes) of found Mon # 28. A line was then established from a point 54.35' northerly from SW Admiral Way along the CL for Fairmount Ave SW (per same records/fieldnotes), and from this point, another line was created southerly through Mon #28 to establish the CL for Fairmount Ave SW, south of SW Admiral Way. The calculated CL for Fairmount Ave SW was then offset 42-feet to establish the right-of-way lines for this street (east line of Block 92 & west line of Block 91).
9. To aid in the determination for the south line of Block 92 (D), SPU survey was able to locate 4 property corners shown on Record of Survey filed under Recording Number 8903249001 (Book 65, Page 84) – these are SPU #'s 1027-1030. The 2 along the easterly margin for Walnut Ave (1027 & 1028) fell within +/- 0.02' of the calc'd line as described above, confirming the calc'd location for that westerly line of Block 92. The 2 points found along the easterly line of this parcel (1029 & 1030) fell east (0.7' & 1.1', respectively) of the calc'd westerly R/W for Fairmount Ave – this was also observed on the Record of Survey filed under Recording Number 20131018900001 (Book 303, PG 6).
10. While point 1029 was not held to determine the easterly line of this parcel, a line between this point and 1027 was held for the "south line" of the parcel, which is also the south line of Lot 4, Block 92. From the intersection of this line with the respective west/east lines for Block 92, the locations for the original SW & SE corners of the Block were established at the Plat distances from the south line of Lot 4 along the respective west/east lines of the Block. The existing/current right-of way for SW Admiral Way was then established per the calls described in City of Seattle Ordinance No. 29062.

NOTE: Holding these calculations as described, the northerly edge of the existing bridge lies just inside the right-of-way at the west end of the bridge, but at the east end of this Block, the bridge is shown over the existing right-of-way by as much as 2.5 feet.

11. This is where problems were then encountered – as it was apparent that there are one, if not more, errors shown on the original plat for the distances along the sidelines for Block 91. The distance along the east lines for all the Lots in this Block were shown at 25', including the east line for Lot 12. However, with the distance for the west line of Lot 12 being longer than all the other Lots in the block (31.01 feet versus 26.11 feet), it would seem to be impossible for this to be the case.
 12. To try and create the original south line for Block 91, the following steps were then taken:
 - Held the 42' O/S from the CL for Fairmount as the westerly line for Block 91.
 - Lacking any evidence to the contrary, the south line for Lot 4, Block 92 was projected easterly, intersecting the easterly right-of-way for Fairmount Ave SW, to create the southwest corner for Lot 4, Block 91.
 - The SW Corner for Block 91 was then calculated at the Plat distance of 213.78', southerly along the east line of Fairmount Ave SW from the calculated SW Corner of Lot 4, Block 91.
 - Finding no monuments to establish the CL for 39th Ave SW north of Admiral Way (and subsequently the east line for Block 91), the SE Corner for Lot 12, Block 91 was calculated from the SW Corner by holding the bearing relationships shown on the Plat for the south lines of Block 91 & 92, and the Plat distance along the south line for Block 91 of 227.81'.
 - The east line for Block 91 was established by projecting the south line for Lot 4 out at the Plat distance of 182.21' and then connecting that with the calculated SE Corner for Block 91.
 13. The “existing right-of-way” for Block 91 was then determined by coming up the respective distances along the west/east lines as called out in City of Seattle Ordinance No. 29062. Looking at the results, this now shows the northerly edge of the SW Admiral Way bridge could lie as much as 8-feet north of the right-of-way line. While I am sure the intent when writing the descriptions in the City Ordinance was to cover the needed area for the bridge footprint, the lack of any monumented ties for the original block corners and the obvious plat errors could result in different locations for the current right-of-way for Block 91, depending on which records are held.
-

Partial list of relevant Field Books:

BK 1768-C, PG 20: Shows distance along Admiral Way “Monument Line” (Random line ran & monumented during historical surveys), between 41st Ave (SPU #57) and Walnut Ave to the South (SPU #56) as 142.46', SPU measured at 142.48'.

- BK 1768-C, PG 19: Shows distance along Admiral Way “Monument Line” (Random line ran & monumented during historical surveys), between 39th Ave to the South (SPU #55) and Fairmount Ave to the South (SPU #28) as 246.06’, SPU measured at 246.76’.
- BK 2817, PG 43: During this field survey, done in 1961, the FB shows that a “Tack in Lead” was set along the Admiral Way “Monument Line”, easterly, at 22.51’ from the MIC at Admiral Way & Walnut Ave (to the South) - SPU located this as #56. To control the alignment for Walnut Ave to the North – this distance was also held for the calculated intersection of the same (Nothing found by SPU at this location), for purposes of this evaluation. This same FB page also shows the CL intersection for Walnut Ave/SW Grayson was established 308.77’ north of Admiral Way & 165.302’ east of 41st St, with a MIC set for this location (SPU located as #59). SPU held the line from #58 to #59 for the CL of SW Grayson (SPU = 164.71’ / FB = 165.00’), and the line from the calculated point along Admiral Way to #59 for the CL of Walnut Ave (SPU = 308.69’ / FB = 308.77’). The sidelines for the ROW (exterior block lines of the plat) were held at a 15’ offset in all instances.

Conclusion:

Based on the review and calculations described above, combined with the lack of any additional records/documentation to resolve the questions raised, it would be advised to first determine the right-of-way needs required along the north side of SW Admiral Way for the desired bridge replacement option. Once the new bridge footprint is identified, then a prospective course of action could be determined to acquire any additional right-of-way that might be required. To resolve the current issues along the north margin, the resulting “new north line for Admiral Way” across these two blocks (91-92) could then be described as an offset from the “monument line” along SW Admiral Way – this is a line that could then be found and easily repeated in the future, helping to avoid any of these issues going forward.

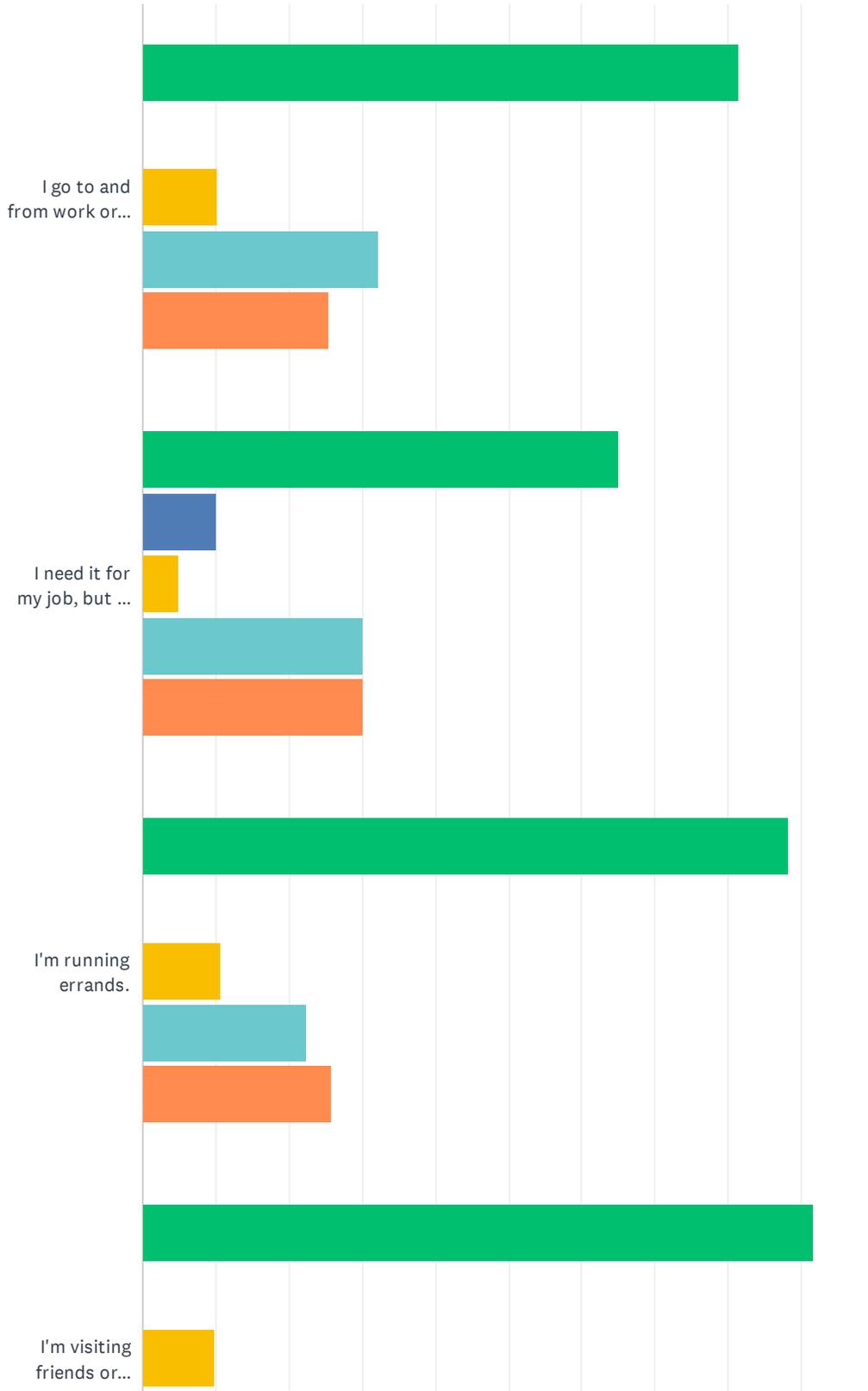
Greg Brooks, PLS

Senior Civil Engineering Specialist
Engineering & Technical Services Division
Project Delivery & Engineering Branch | Seattle Public Utilities
(206) 615-1091 / Desk | (206) 643-0956 / Cell | Cube #4717B
Email: gregory.brooks@seattle.gov

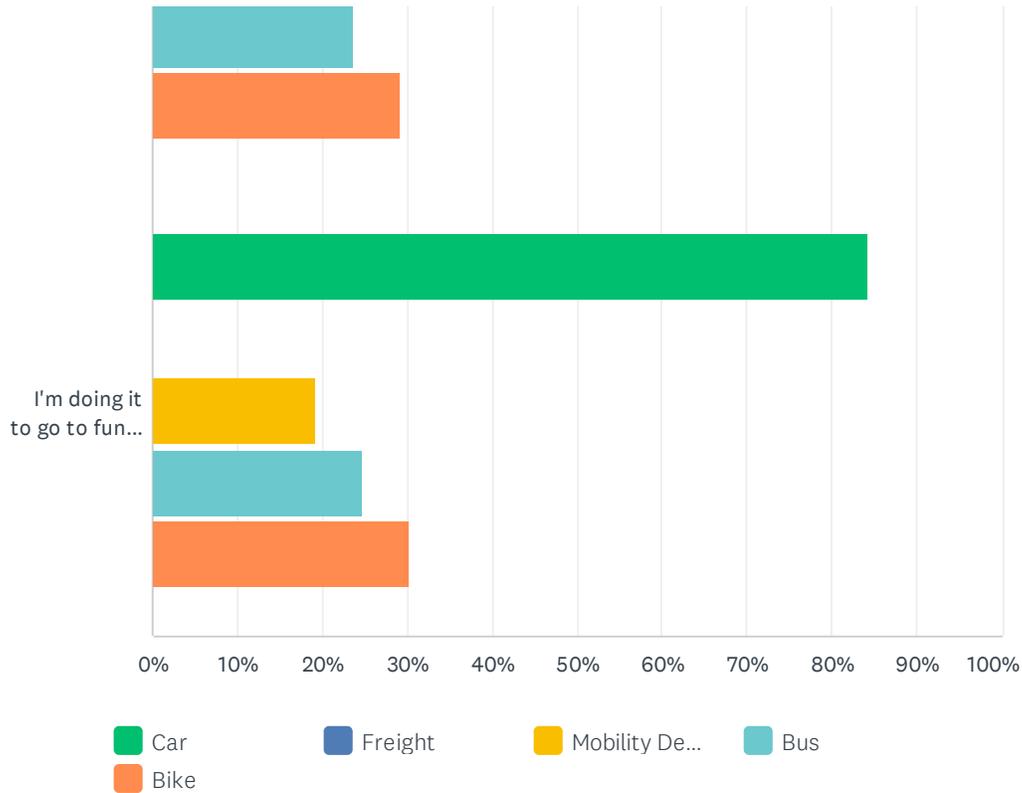
Appendix I. Public Outreach Survey Results

Q1 Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

Answered: 94 Skipped: 1



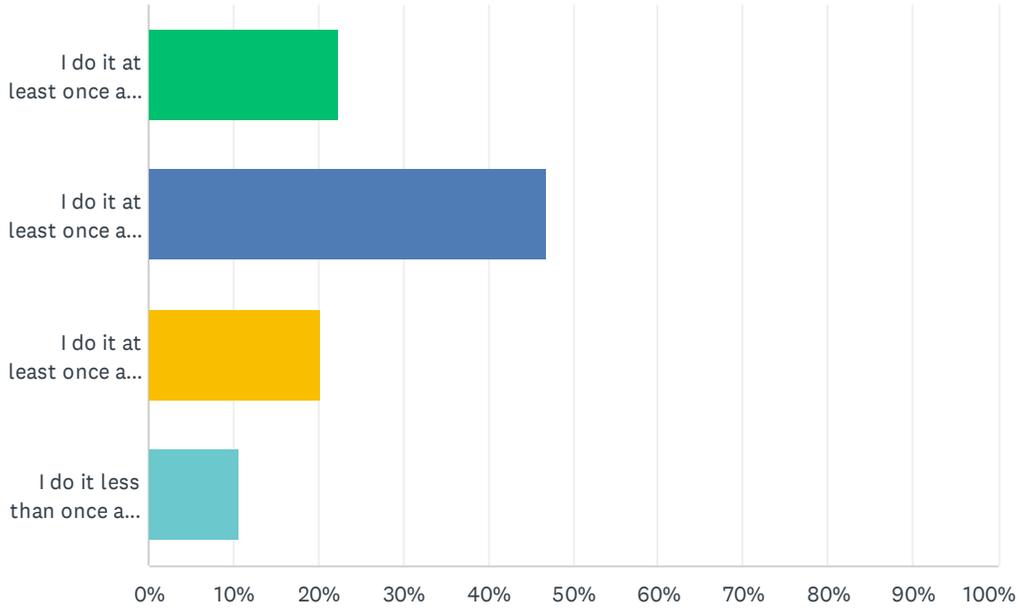
Admiral Way SW Bridges Public Survey



	CAR	FREIGHT	MOBILITY DEVICE/WALKING	BUS	BIKE	TOTAL RESPONDENTS
I go to and from work or school.	81.36% 48	0.00% 0	10.17% 6	32.20% 19	25.42% 15	59
I need it for my job, but not for getting to work (like freight, deliveries, rideshare, etc.)	65.00% 13	10.00% 2	5.00% 1	30.00% 6	30.00% 6	20
I'm running errands.	88.24% 75	0.00% 0	10.59% 9	22.35% 19	25.88% 22	85
I'm visiting friends or family.	91.67% 66	0.00% 0	9.72% 7	23.61% 17	29.17% 21	72
I'm doing it to go to fun activities.	84.27% 75	0.00% 0	19.10% 17	24.72% 22	30.34% 27	89

Q2 How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

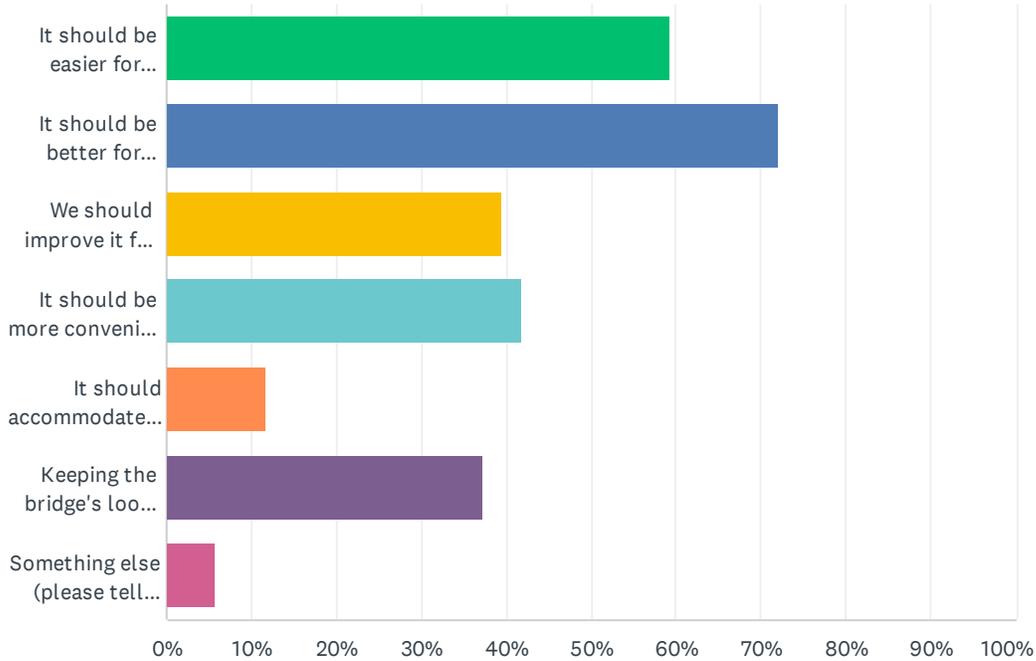
Answered: 94 Skipped: 1



ANSWER CHOICES	RESPONSES	
I do it at least once a day.	22.34%	21
I do it at least once a week.	46.81%	44
I do it at least once a month.	20.21%	19
I do it less than once a month.	10.64%	10
TOTAL		94

Q3 Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

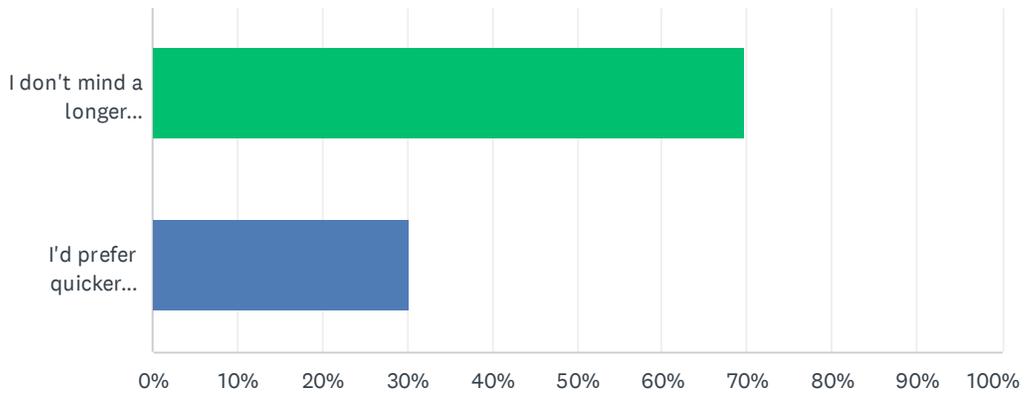
Answered: 86 Skipped: 9



ANSWER CHOICES	RESPONSES	
It should be easier for people biking.	59.30%	51
It should be better for people walking.	72.09%	62
We should improve it for people driving.	39.53%	34
It should be more convenient for people who use public transport.	41.86%	36
It should accommodate freight better.	11.63%	10
Keeping the bridge's look or its historical features is important.	37.21%	32
Something else (please tell us more).	5.81%	5
Total Respondents: 86		

Q4 How about the construction timeline – which would you prefer?

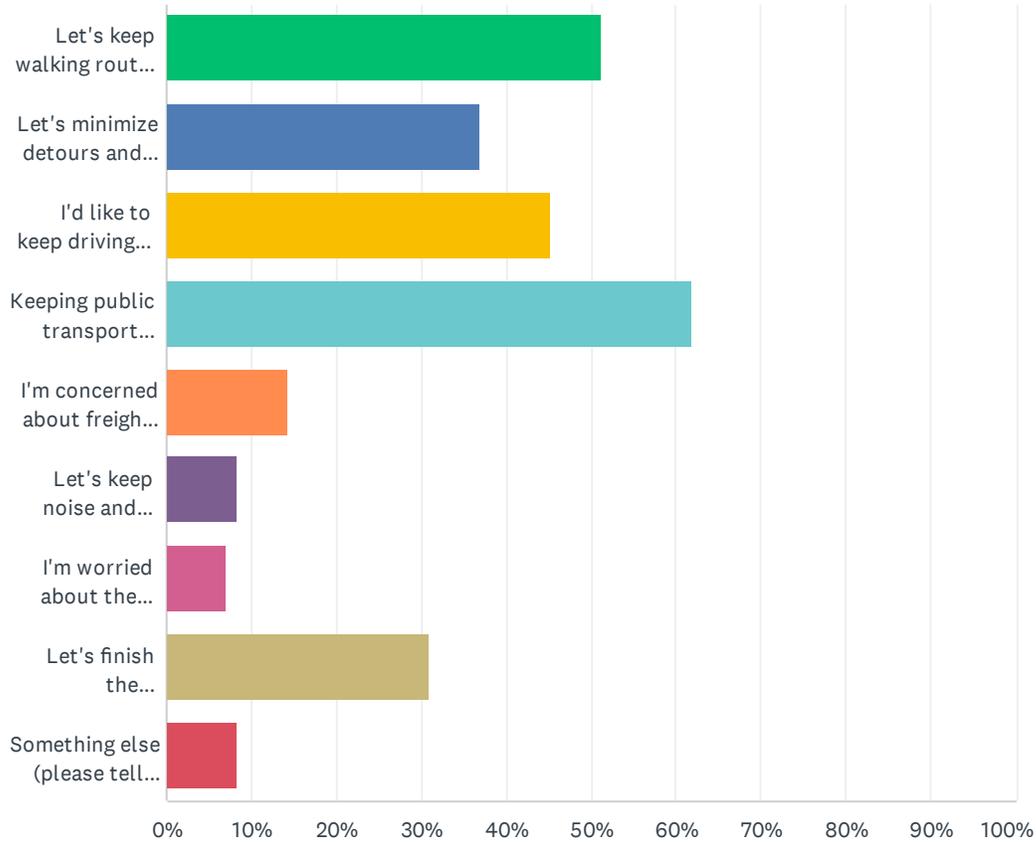
Answered: 86 Skipped: 9



ANSWER CHOICES	RESPONSES	
I don't mind a longer construction period if I can still use part of the bridge.	69.77%	60
I'd prefer quicker construction, even if it means no bridge access during that time.	30.23%	26
TOTAL		86

Q5 Construction will bring some changes. What are your top three concerns from the list below?

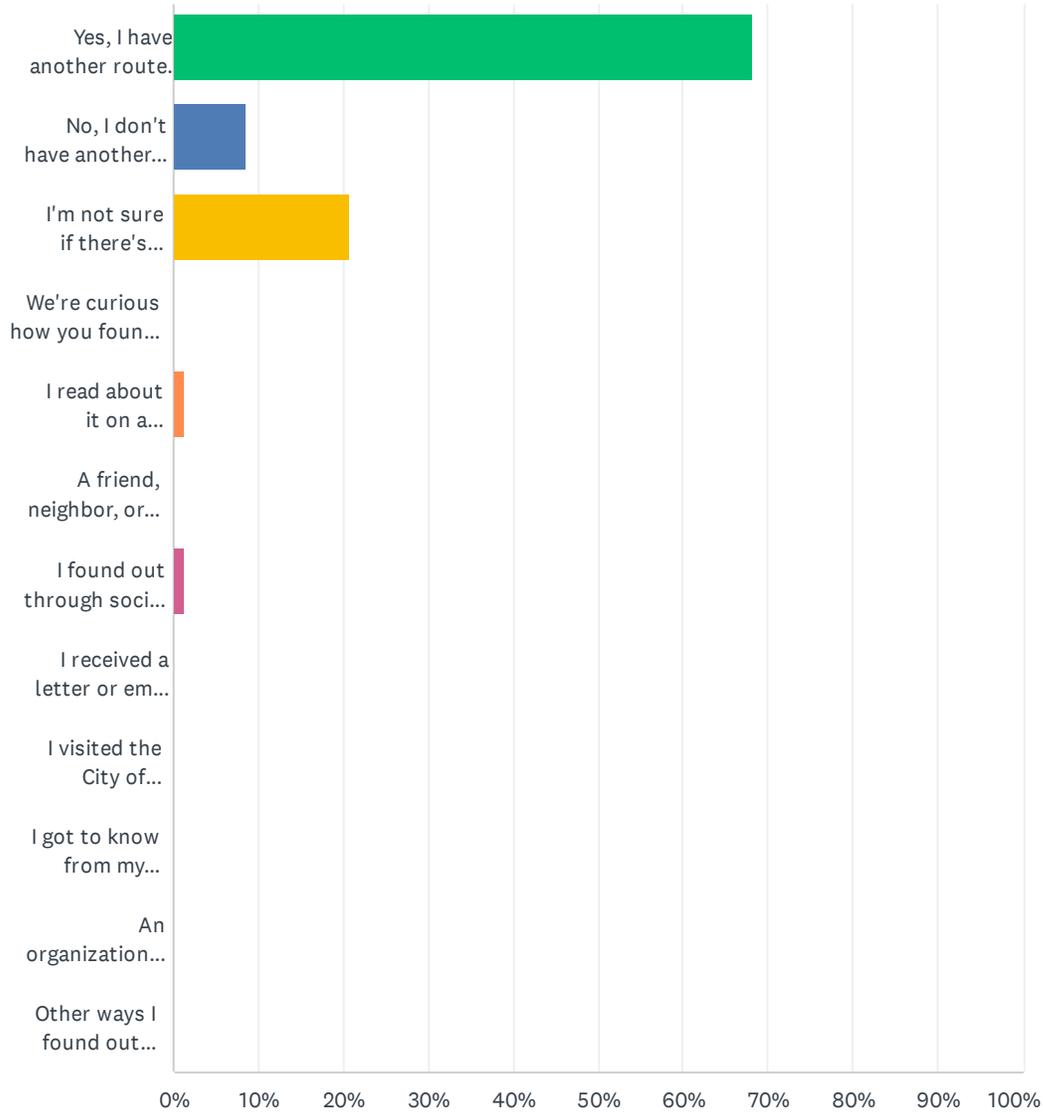
Answered: 84 Skipped: 11



ANSWER CHOICES	RESPONSES	
Let's keep walking routes open and detour-free.	51.19%	43
Let's minimize detours and closures for cyclists.	36.90%	31
I'd like to keep driving routes clear.	45.24%	38
Keeping public transport routes open is important.	61.90%	52
I'm concerned about freight traffic detours or closures.	14.29%	12
Let's keep noise and shaking to a minimum.	8.33%	7
I'm worried about the impact on property nearby.	7.14%	6
Let's finish the construction as quickly as possible.	30.95%	26
Something else (please tell us more).	8.33%	7
Total Respondents: 84		

Q6 If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Answered: 82 Skipped: 13

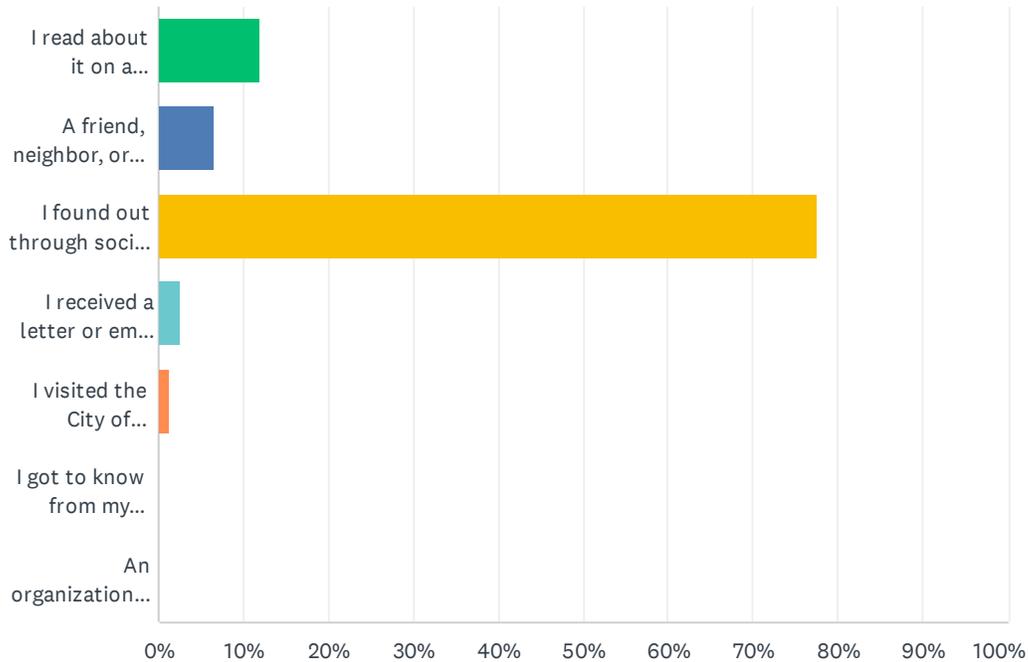


Admiral Way SW Bridges Public Survey

ANSWER CHOICES	RESPONSES	
Yes, I have another route.	68.29%	56
No, I don't have another route.	8.54%	7
I'm not sure if there's another route.	20.73%	17
We're curious how you found out about this project. Can you let us know?	0.00%	0
I read about it on a neighborhood blog.	1.22%	1
A friend, neighbor, or family member told me.	0.00%	0
I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)	1.22%	1
I received a letter or email from the City of Seattle/SDOT.	0.00%	0
I visited the City of Seattle/SDOT website.	0.00%	0
I got to know from my employer.	0.00%	0
An organization I'm part of mentioned it.	0.00%	0
Other ways I found out (please share with us).	0.00%	0
TOTAL		82

Q7 We're curious how you found out about this project. Can you let us know?

Answered: 76 Skipped: 19



ANSWER CHOICES	RESPONSES	
I read about it on a neighborhood blog.	11.84%	9
A friend, neighbor, or family member told me.	6.58%	5
I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)	77.63%	59
I received a letter or email from the City of Seattle/SDOT.	2.63%	2
I visited the City of Seattle/SDOT website.	1.32%	1
I got to know from my employer.	0.00%	0
An organization I'm part of mentioned it.	0.00%	0
TOTAL		76

Public Outreach Survey Responses (95 total)

#1

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 4:45:36 PM
Last Modified: Wednesday, January 31, 2024 4:47:10 PM
Time Spent: 00:01:33
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Bus, Bike
I'm running errands.	Bus, Bike
I'm doing it to go to fun activities.	Bike

Q2**I do it at least once a week.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
It should be better for people walking. ,
It should be more convenient for people who use public transport.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.**Q5**

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,
Let's minimize detours and closures for cyclists. ,
Keeping public transport routes open is important.

Page 4: Follow-Up Question

Q6

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

Q7

We're curious how you found out about this project. Can you let us know?

Respondent skipped this question

#2

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 4:51:45 PM
Last Modified: Wednesday, January 31, 2024 4:53:23 PM
Time Spent: 00:01:37
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I need it for my job, but not for getting to work (like freight, deliveries, rideshare, etc.)	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it at least once a day.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

We should improve it for people driving. ,
It should accommodate freight better. ,
Keeping the bridge's look or its historical features is important.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

I'd like to keep driving routes clear. ,
I'm concerned about freight traffic detours or closures. ,
I'm worried about the impact on property nearby.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

Respondent skipped this question

We're curious how you found out about this project. Can you let us know?

#3

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 5:09:00 PM
Last Modified: Wednesday, January 31, 2024 5:11:36 PM
Time Spent: 00:02:35
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands.

Bike

I'm doing it to go to fun activities.

Mobility Device/Walking, Bike

Q2

I do it at least once a month.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,

It should be better for people walking. ,

It should be more convenient for people who use public transport.

Q4

How about the construction timeline – which would you prefer?

I'd prefer quicker construction, even if it means no bridge access during that time.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,

Let's minimize detours and closures for cyclists. ,

Keeping public transport routes open is important.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

Respondent skipped this question

We're curious how you found out about this project. Can you let us know?

#4

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 5:16:54 PM
Last Modified: Wednesday, January 31, 2024 5:18:20 PM
Time Spent: 00:01:25
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it at least once a month.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be better for people walking. ,
We should improve it for people driving. ,
Keeping the bridge's look or its historical features is important.

Q4

How about the construction timeline – which would you prefer?

I'd prefer quicker construction, even if it means no bridge access during that time.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,
Let's finish the construction as quickly as possible.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

Respondent skipped this question

We're curious how you found out about this project. Can you let us know?

#5

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 5:14:39 PM
Last Modified: Wednesday, January 31, 2024 5:18:34 PM
Time Spent: 00:03:54
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2**I do it at least once a week.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3**It should be easier for people biking. ,**

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be better for people walking.**Q4****I'd prefer quicker construction, even if it means no bridge access during that time.**

How about the construction timeline – which would you prefer?

Q5**Let's finish the construction as quickly as possible.**

Construction will bring some changes. What are your top three concerns from the list below?

Page 4: Follow-Up Question

Q6

Respondent skipped this question

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

Respondent skipped this question

We're curious how you found out about this project. Can you let us know?

#6

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 5:16:27 PM
Last Modified: Wednesday, January 31, 2024 5:20:04 PM
Time Spent: 00:03:36
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.

Car

I'm running errands.

Car, Mobility Device/Walking

I'm doing it to go to fun activities.

Car, Mobility Device/Walking, Bus

Q2

I do it at least once a week.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,

It should be better for people walking. ,

Keeping the bridge's look or its historical features is important.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,

I'd like to keep driving routes clear. ,

Let's finish the construction as quickly as possible.

Page 4: Follow-Up Question

Q6

I'm not sure if there's another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

Respondent skipped this question

We're curious how you found out about this project. Can you let us know?

#7

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 5:16:32 PM
Last Modified: Wednesday, January 31, 2024 5:20:19 PM
Time Spent: 00:03:47
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it at least once a week.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be better for people walking. ,
We should improve it for people driving. ,
It should accommodate freight better.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

I'd like to keep driving routes clear. ,
I'm concerned about freight traffic detours or closures. ,
Let's finish the construction as quickly as possible.

Page 4: Follow-Up Question

Q6

I read about it on a neighborhood blog.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

Respondent skipped this question

We're curious how you found out about this project. Can you let us know?

#8

INCOMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 5:24:17 PM
Last Modified: Wednesday, January 31, 2024 5:25:58 PM
Time Spent: 00:01:41
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.

Car

I need it for my job, but not for getting to work (like freight, deliveries, rideshare, etc.)

Freight

I'm running errands.

Mobility Device/Walking

Q2

I do it at least once a month.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

It should be easier for people biking. ,

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

We should improve it for people driving.

Q4

I'd prefer quicker construction, even if it means no bridge access during that time.

How about the construction timeline – which would you prefer?

Q5

I'd like to keep driving routes clear.

Construction will bring some changes. What are your top three concerns from the list below?

Page 4: Follow-Up Question

Q6

Respondent skipped this question

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

Respondent skipped this question

We're curious how you found out about this project. Can you let us know?

#9

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 5:26:57 PM
Last Modified: Wednesday, January 31, 2024 5:28:25 PM
Time Spent: 00:01:28
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Mobility Device/Walking, Bus, Bike
I need it for my job, but not for getting to work (like freight, deliveries, rideshare, etc.)	Bus, Bike
I'm running errands.	Bus, Bike
I'm visiting friends or family.	Mobility Device/Walking, Bus, Bike
I'm doing it to go to fun activities.	Mobility Device/Walking, Bus, Bike

Q2**I do it at least once a week.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
It should be better for people walking. ,
It should be more convenient for people who use public transport.

Q4

How about the construction timeline – which would you prefer?

I'd prefer quicker construction, even if it means no bridge access during that time.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,
Let's minimize detours and closures for cyclists. ,
Keeping public transport routes open is important.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

A friend, neighbor, or family member told me.

We're curious how you found out about this project. Can you let us know?

#10

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 5:30:55 PM
Last Modified: Wednesday, January 31, 2024 5:33:41 PM
Time Spent: 00:02:45
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands.	Car
I'm visiting friends or family.	Car, Bike
I'm doing it to go to fun activities.	Car, Bike

Q2

I do it at least once a month.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
It should be better for people walking. ,
It should be more convenient for people who use public transport.

Q4

How about the construction timeline – which would you prefer?

I'd prefer quicker construction, even if it means no bridge access during that time.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,
Keeping public transport routes open is important. ,
Let's finish the construction as quickly as possible.

Page 4: Follow-Up Question

Q6 **I'm not sure if there's another route.**

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7 **I received a letter or email from the City of Seattle/SDOT.**

We're curious how you found out about this project. Can you let us know?

#11

INCOMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 5:39:11 PM
Last Modified: Wednesday, January 31, 2024 5:39:18 PM
Time Spent: 00:00:06
Language: Amharic

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I need it for my job, but not for getting to work (like freight, deliveries, rideshare, etc.) **Car**

Q2

Respondent skipped this question

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Respondent skipped this question

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Q4

Respondent skipped this question

How about the construction timeline – which would you prefer?

Q5

Respondent skipped this question

Construction will bring some changes. What are your top three concerns from the list below?

Page 4: Follow-Up Question

Q6

Respondent skipped this question

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

Respondent skipped this question

We're curious how you found out about this project. Can you let us know?

#12

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 5:38:31 PM
Last Modified: Wednesday, January 31, 2024 5:40:30 PM
Time Spent: 00:01:58
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it at least once a week.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

We should improve it for people driving. ,
 It should accommodate freight better. ,
 Keeping the bridge's look or its historical features is important.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

I'd like to keep driving routes clear. ,
 I'm concerned about freight traffic detours or closures. ,
 Let's keep noise and shaking to a minimum.

Page 4: Follow-Up Question

Q6

I'm not sure if there's another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#13

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 5:59:58 PM
Last Modified: Wednesday, January 31, 2024 6:01:53 PM
Time Spent: 00:01:54
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands.	Bike
I'm visiting friends or family.	Bus, Bike
I'm doing it to go to fun activities.	Bike

Q2**I do it at least once a week.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
It should be better for people walking. ,
It should be more convenient for people who use public transport.

Q4

How about the construction timeline – which would you prefer?

I'd prefer quicker construction, even if it means no bridge access during that time.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,
Let's minimize detours and closures for cyclists. ,
Keeping public transport routes open is important.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#14

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 6:25:49 PM
Last Modified: Wednesday, January 31, 2024 6:28:32 PM
Time Spent: 00:02:43
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I need it for my job, but not for getting to work (like freight, deliveries, rideshare, etc.)	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it at least once a week.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
It should be better for people walking. ,
It should be more convenient for people who use public transport.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's minimize detours and closures for cyclists. ,
Keeping public transport routes open is important. ,
Let's keep noise and shaking to a minimum.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I read about it on a neighborhood blog.

We're curious how you found out about this project. Can you let us know?

#15

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 6:30:18 PM
Last Modified: Wednesday, January 31, 2024 6:33:33 PM
Time Spent: 00:03:15
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I'm running errands.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it less than once a month.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
It should be better for people walking. ,
We should improve it for people driving.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,
Let's minimize detours and closures for cyclists. ,
I'd like to keep driving routes clear.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I read about it on a neighborhood blog.

We're curious how you found out about this project. Can you let us know?

#16

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 6:49:48 PM
Last Modified: Wednesday, January 31, 2024 6:53:32 PM
Time Spent: 00:03:44
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car, Mobility Device/Walking

Q2**I do it at least once a week.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3**We should improve it for people driving.**

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Q4**I don't mind a longer construction period if I can still use part of the bridge.**

How about the construction timeline – which would you prefer?

Q5**I'd like to keep driving routes clear.**

Construction will bring some changes. What are your top three concerns from the list below?

Page 4: Follow-Up Question

Q6

No, I don't have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I read about it on a neighborhood blog.

We're curious how you found out about this project. Can you let us know?

#17

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 7:04:39 PM
Last Modified: Wednesday, January 31, 2024 7:07:31 PM
Time Spent: 00:02:52
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it at least once a day.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
It should be better for people walking. ,
It should be more convenient for people who use public transport.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,
Let's minimize detours and closures for cyclists. ,
I'd like to keep driving routes clear.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#18

INCOMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 5:11:47 PM
Last Modified: Wednesday, January 31, 2024 7:25:20 PM
Time Spent: 02:13:32
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands.

Car

I'm doing it to go to fun activities.

Car, Mobility Device/Walking

Another reason (please tell us).

Exercise (Running)

Q2

I do it at least once a week.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Respondent skipped this question

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Q4

Respondent skipped this question

How about the construction timeline – which would you prefer?

Q5

Respondent skipped this question

Construction will bring some changes. What are your top three concerns from the list below?

Page 4: Follow-Up Question

Q6

Respondent skipped this question

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

Respondent skipped this question

We're curious how you found out about this project. Can you let us know?

#19

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 7:57:00 PM
Last Modified: Wednesday, January 31, 2024 8:05:45 PM
Time Spent: 00:08:44
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands.	Car, Bus, Bike
I'm visiting friends or family.	Car, Bus, Bike
I'm doing it to go to fun activities.	Car, Bus, Bike

Q2

I do it at least once a month.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
It should be better for people walking. ,
It should be more convenient for people who use public transport.

Q4

Respondent skipped this question

How about the construction timeline – which would you prefer?

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's minimize detours and closures for cyclists. ,
Keeping public transport routes open is important. ,
 Something else (please tell us more). :
 It's hard to answer whether I'd prefer a short full closure vs a long partial one without knowing the ballpark lengths

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

We're curious how you found out about this project. Can you let us know?

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

,

Other ways I found out (please share with us). :

I'm pretty connected so I'll likely find out through SDOT email, neighborhood blog and an organization soon

#20

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 8:49:49 PM
Last Modified: Wednesday, January 31, 2024 8:51:39 PM
Time Spent: 00:01:49
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm doing it to go to fun activities.

Bus

Q2

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

I do it less than once a month.

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,

It should be more convenient for people who use public transport.

,

Keeping the bridge's look or its historical features is important.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's minimize detours and closures for cyclists. ,

Keeping public transport routes open is important. ,

I'm concerned about freight traffic detours or closures.

Page 4: Follow-Up Question

Q6

I'm not sure if there's another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#21

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 8:51:18 PM
Last Modified: Wednesday, January 31, 2024 8:55:37 PM
Time Spent: 00:04:18
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it at least once a week.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Keeping the bridge's look or its historical features is important.

'
 Something else (please tell us more). :

The design should help slow traffic, people drive too fast on that road

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

I'd like to keep driving routes clear. ,

Something else (please tell us more). :

Just get it done and keep the costs and political process down

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

,
Other ways I found out (please share with us). :
In Threads.

#22

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 8:57:13 PM
Last Modified: Wednesday, January 31, 2024 8:59:30 PM
Time Spent: 00:02:17
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car, Bike
I'm visiting friends or family.	Car, Bike
I'm doing it to go to fun activities.	Car

Q2**I do it at least once a month.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
We should improve it for people driving. ,
It should accommodate freight better.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,
Let's minimize detours and closures for cyclists. ,
I'm concerned about freight traffic detours or closures.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#23

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 9:00:10 PM
Last Modified: Wednesday, January 31, 2024 9:01:46 PM
Time Spent: 00:01:35
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2**I do it at least once a week.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3**Keeping the bridge's look or its historical features is important.**

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Q4**I don't mind a longer construction period if I can still use part of the bridge.**

How about the construction timeline – which would you prefer?

Q5**Keeping public transport routes open is important. ,
I'm concerned about freight traffic detours or closures.**

Construction will bring some changes. What are your top three concerns from the list below?

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#24

INCOMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 9:02:50 PM
Last Modified: Wednesday, January 31, 2024 9:03:33 PM
Time Spent: 00:00:42
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands. **Car**

I'm doing it to go to fun activities. **Car**

Q2

I do it at least once a week.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Respondent skipped this question

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Q4

Respondent skipped this question

How about the construction timeline – which would you prefer?

Q5

Respondent skipped this question

Construction will bring some changes. What are your top three concerns from the list below?

Page 4: Follow-Up Question

Q6

Respondent skipped this question

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

Respondent skipped this question

We're curious how you found out about this project. Can you let us know?

#25

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 9:45:51 PM
Last Modified: Wednesday, January 31, 2024 9:47:55 PM
Time Spent: 00:02:03
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I need it for my job, but not for getting to work (like freight, deliveries, rideshare, etc.)	Car, Bus, Bike
I'm running errands.	Car, Bus, Bike
I'm visiting friends or family.	Car, Bus, Bike
I'm doing it to go to fun activities.	Car, Bus

Q2**I do it at least once a day.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
We should improve it for people driving. ,
Keeping the bridge's look or its historical features is important.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

I'd like to keep driving routes clear. ,
Let's finish the construction as quickly as possible.

Page 4: Follow-Up Question

Q6

No, I don't have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I read about it on a neighborhood blog.

We're curious how you found out about this project. Can you let us know?

#26

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 9:45:33 PM
Last Modified: Wednesday, January 31, 2024 9:48:49 PM
Time Spent: 00:03:15
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Mobility Device/Walking
I need it for my job, but not for getting to work (like freight, deliveries, rideshare, etc.)	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2**I do it at least once a day.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3**It should be better for people walking.**

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Q4**I don't mind a longer construction period if I can still use part of the bridge.**

How about the construction timeline – which would you prefer?

Q5**I'd like to keep driving routes clear.**

Construction will bring some changes. What are your top three concerns from the list below?

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#27

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 10:17:07 PM
Last Modified: Wednesday, January 31, 2024 10:21:08 PM
Time Spent: 00:04:00
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm doing it to go to fun activities.

Car

Q2

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

I do it less than once a month.

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,

It should be better for people walking. ,

It should be more convenient for people who use public transport.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Respondent skipped this question

Page 4: Follow-Up Question

Q6

I'm not sure if there's another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#28

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 10:39:05 PM
Last Modified: Wednesday, January 31, 2024 10:41:47 PM
Time Spent: 00:02:42
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.

Car, Bus, Bike

I'm running errands.

Car, Bike

I'm visiting friends or family.

Car, Mobility Device/Walking, Bike

I'm doing it to go to fun activities.

Car, Mobility Device/Walking, Bus, Bike

Q2

I do it at least once a month.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,

It should be better for people walking. ,

Keeping the bridge's look or its historical features is important.

Q4

How about the construction timeline – which would you prefer?

I'd prefer quicker construction, even if it means no bridge access during that time.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's minimize detours and closures for cyclists. ,

Keeping public transport routes open is important.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#29

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 10:41:08 PM
Last Modified: Wednesday, January 31, 2024 10:42:27 PM
Time Spent: 00:01:18
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.

Car, Mobility Device/Walking, Bus, Bike

I'm running errands.

Car, Bus, Bike

I'm visiting friends or family.

Car, Bus, Bike

I'm doing it to go to fun activities.

Car, Bus, Bike

Q2

I do it at least once a week.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,

It should be better for people walking. ,

We should improve it for people driving.

Q4

How about the construction timeline – which would you prefer?

I'd prefer quicker construction, even if it means no bridge access during that time.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's minimize detours and closures for cyclists. ,

I'd like to keep driving routes clear. ,

Keeping public transport routes open is important.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#30

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 10:52:32 PM
Last Modified: Wednesday, January 31, 2024 10:55:21 PM
Time Spent: 00:02:49
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands.	Car, Mobility Device/Walking, Bike
I'm visiting friends or family.	Car, Mobility Device/Walking, Bike
I'm doing it to go to fun activities.	Car, Mobility Device/Walking, Bike

Q2

I do it at least once a week.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
It should be better for people walking. ,
We should improve it for people driving.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,
I'd like to keep driving routes clear. ,
I'm worried about the impact on property nearby.

Page 4: Follow-Up Question

Q6 **I'm not sure if there's another route.**

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7 **I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)**

We're curious how you found out about this project. Can you let us know?

#31

INCOMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 11:27:56 PM
Last Modified: Wednesday, January 31, 2024 11:28:58 PM
Time Spent: 00:01:01
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it less than once a month.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Respondent skipped this question

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Q4

Respondent skipped this question

How about the construction timeline – which would you prefer?

Q5

Respondent skipped this question

Construction will bring some changes. What are your top three concerns from the list below?

Page 4: Follow-Up Question

Q6

Respondent skipped this question

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

Respondent skipped this question

We're curious how you found out about this project. Can you let us know?

#32

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Wednesday, January 31, 2024 11:42:50 PM
Last Modified: Wednesday, January 31, 2024 11:45:16 PM
Time Spent: 00:02:26
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands. **Car, Bike**

I'm doing it to go to fun activities. **Car, Bike**

Q2 **I do it at least once a month.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,

It should be better for people walking. ,

It should be more convenient for people who use public transport.

,

It should accommodate freight better.

Q4

How about the construction timeline – which would you prefer?

I'd prefer quicker construction, even if it means no bridge access during that time.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Respondent skipped this question

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

A friend, neighbor, or family member told me.

We're curious how you found out about this project. Can you let us know?

#33

INCOMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Thursday, February 01, 2024 12:01:39 AM
Last Modified: Thursday, February 01, 2024 12:02:37 AM
Time Spent: 00:00:58
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I need it for my job, but not for getting to work (like freight, deliveries, rideshare, etc.)	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it at least once a day.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Respondent skipped this question

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Q4

Respondent skipped this question

How about the construction timeline – which would you prefer?

Q5

Respondent skipped this question

Construction will bring some changes. What are your top three concerns from the list below?

Page 4: Follow-Up Question

Q6 **Respondent skipped this question**

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7 **Respondent skipped this question**

We're curious how you found out about this project. Can you let us know?

#34

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Thursday, February 01, 2024 12:19:40 AM
Last Modified: Thursday, February 01, 2024 12:21:50 AM
Time Spent: 00:02:10
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it at least once a week.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be better for people walking. ,
It should be more convenient for people who use public transport.
Keeping the bridge's look or its historical features is important.

Q4

How about the construction timeline – which would you prefer?

I'd prefer quicker construction, even if it means no bridge access during that time.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Keeping public transport routes open is important. ,
I'm worried about the impact on property nearby. ,
Let's finish the construction as quickly as possible.

Page 4: Follow-Up Question

Q6 **I'm not sure if there's another route.**

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7 **I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)**

We're curious how you found out about this project. Can you let us know?

#35

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Thursday, February 01, 2024 12:45:26 AM
Last Modified: Thursday, February 01, 2024 12:50:28 AM
Time Spent: 00:05:02
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it at least once a month.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
It should be better for people walking. ,
Keeping the bridge's look or its historical features is important.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,
Let's minimize detours and closures for cyclists. ,
 Something else (please tell us more). :
 Impact on side streets if cars are blocked or detoured

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#36

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Thursday, February 01, 2024 1:54:08 AM
Last Modified: Thursday, February 01, 2024 1:57:25 AM
Time Spent: 00:03:17
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.

Bus

I'm running errands.

Car, Bus

I'm visiting friends or family.

Car

Q2

I do it at least once a month.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,

It should be better for people walking. ,

It should be more convenient for people who use public transport.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Keeping public transport routes open is important. ,

Something else (please tell us more). :

Keep Fairmount Ave open under the bridges

Page 4: Follow-Up Question

Q6 **I'm not sure if there's another route.**

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7 **I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)**

We're curious how you found out about this project. Can you let us know?

#37

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Thursday, February 01, 2024 2:10:18 AM
Last Modified: Thursday, February 01, 2024 2:11:29 AM
Time Spent: 00:01:11
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I need it for my job, but not for getting to work (like freight, deliveries, rideshare, etc.)	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it at least once a day.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

We should improve it for people driving.

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Q4

I don't mind a longer construction period if I can still use part of the bridge.

How about the construction timeline – which would you prefer?

Q5

I'd like to keep driving routes clear.

Construction will bring some changes. What are your top three concerns from the list below?

Page 4: Follow-Up Question

Q6 **No, I don't have another route.**

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7 **I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)**

We're curious how you found out about this project. Can you let us know?

#38

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Thursday, February 01, 2024 3:54:24 AM
Last Modified: Thursday, February 01, 2024 3:56:33 AM
Time Spent: 00:02:08
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Bike
I'm running errands.	Car, Bike
I'm visiting friends or family.	Car, Bike
I'm doing it to go to fun activities.	Car, Bike

Q2**I do it at least once a week.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3**It should be easier for people biking. ,**

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Keeping the bridge's look or its historical features is important.**Q4****I don't mind a longer construction period if I can still use part of the bridge.**

How about the construction timeline – which would you prefer?

Q5**Respondent skipped this question**

Construction will bring some changes. What are your top three concerns from the list below?

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#39

INCOMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Thursday, February 01, 2024 7:03:41 AM
Last Modified: Thursday, February 01, 2024 7:04:28 AM
Time Spent: 00:00:47
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I need it for my job, but not for getting to work (like freight, deliveries, rideshare, etc.)	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it less than once a month.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Respondent skipped this question

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Q4

Respondent skipped this question

How about the construction timeline – which would you prefer?

Q5

Respondent skipped this question

Construction will bring some changes. What are your top three concerns from the list below?

Page 4: Follow-Up Question

Q6 **Respondent skipped this question**

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7 **Respondent skipped this question**

We're curious how you found out about this project. Can you let us know?

#40

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Thursday, February 01, 2024 7:39:33 AM
Last Modified: Thursday, February 01, 2024 7:43:34 AM
Time Spent: 00:04:00
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands.	Car, Bus, Bike
I'm visiting friends or family.	Car, Bus
I'm doing it to go to fun activities.	Car, Bus

Q2**I do it at least once a week.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3**It should be better for people walking.**

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Q4**I don't mind a longer construction period if I can still use part of the bridge.**

How about the construction timeline – which would you prefer?

Q5

Let's keep walking routes open and detour-free. ,
Let's minimize detours and closures for cyclists. ,
Keeping public transport routes open is important. ,
Let's finish the construction as quickly as possible.

Construction will bring some changes. What are your top three concerns from the list below?

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#41

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Thursday, February 01, 2024 8:12:08 AM
Last Modified: Thursday, February 01, 2024 8:13:29 AM
Time Spent: 00:01:21
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

- I go to and from work or school. **Bus, Bike**
 - I need it for my job, but not for getting to work (like freight, deliveries, rideshare, etc.) **Bus, Bike**
 - I'm running errands. **Bus, Bike**
 - I'm visiting friends or family. **Bus, Bike**
 - I'm doing it to go to fun activities. **Bus, Bike**
-

Q2

I do it at least once a day.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

- It should be easier for people biking. ,**
 - It should be better for people walking. ,**
 - It should be more convenient for people who use public transport.**
 - ,**
 - Keeping the bridge's look or its historical features is important.**
-

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,
Let's minimize detours and closures for cyclists. ,
Keeping public transport routes open is important.

Page 4: Follow-Up Question

Q6

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

No, I don't have another route.

Q7

We're curious how you found out about this project. Can you let us know?

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

#42

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Thursday, February 01, 2024 11:38:53 AM
Last Modified: Thursday, February 01, 2024 11:41:51 AM
Time Spent: 00:02:58
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it less than once a month.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

We should improve it for people driving. ,
 It should accommodate freight better. ,
 Keeping the bridge's look or its historical features is important.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

I'd like to keep driving routes clear. ,
 Keeping public transport routes open is important. ,
 I'm concerned about freight traffic detours or closures. ,
 Let's keep noise and shaking to a minimum.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#43

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Thursday, February 01, 2024 4:23:34 PM
Last Modified: Thursday, February 01, 2024 4:25:31 PM
Time Spent: 00:01:56
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car
Another reason (please tell us). Local commerce like grocery shopping	

Q2

I do it at least once a week.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

We should improve it for people driving. ,

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Keeping the bridge's look or its historical features is important.

Q4

I don't mind a longer construction period if I can still use part of the bridge.

How about the construction timeline – which would you prefer?

Q5

Let's keep walking routes open and detour-free. ,

Construction will bring some changes. What are your top three concerns from the list below?

I'd like to keep driving routes clear. ,

Keeping public transport routes open is important.

Page 4: Follow-Up Question

Q6 **I'm not sure if there's another route.**

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7 **I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)**

We're curious how you found out about this project. Can you let us know?

#44

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Thursday, February 01, 2024 5:38:59 PM
Last Modified: Thursday, February 01, 2024 5:40:29 PM
Time Spent: 00:01:29
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car, Bus
I need it for my job, but not for getting to work (like freight, deliveries, rideshare, etc.)	Car, Bus
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it at least once a day.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Keeping the bridge's look or its historical features is important.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,

I'd like to keep driving routes clear. ,

Keeping public transport routes open is important.

Page 4: Follow-Up Question

Q6 **I'm not sure if there's another route.**

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7 **I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)**

We're curious how you found out about this project. Can you let us know?

#45

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Thursday, February 01, 2024 9:41:39 PM
Last Modified: Thursday, February 01, 2024 9:43:13 PM
Time Spent: 00:01:33
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car, Bus
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2**I do it at least once a day.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
It should be better for people walking. ,
It should be more convenient for people who use public transport.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,
Let's minimize detours and closures for cyclists. ,
Keeping public transport routes open is important.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#46

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Thursday, February 01, 2024 10:57:00 PM
Last Modified: Thursday, February 01, 2024 10:59:57 PM
Time Spent: 00:02:57
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands.

Car, Bike

I'm doing it to go to fun activities.

Bike

Q2

I do it at least once a month.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,**It should be better for people walking. ,****It should be more convenient for people who use public transport.**

,

Something else (please tell us more). :

Quiet and protected underneath

Q4

How about the construction timeline – which would you prefer?

I'd prefer quicker construction, even if it means no bridge access during that time.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's minimize detours and closures for cyclists. ,**Keeping public transport routes open is important. ,**

Something else (please tell us more). :

Minimal damage to the area below

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#47

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Friday, February 02, 2024 11:45:49 AM
Last Modified: Friday, February 02, 2024 11:48:58 AM
Time Spent: 00:03:09
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it less than once a month.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
It should be better for people walking. ,
We should improve it for people driving. ,
It should be more convenient for people who use public transport.

Q4

How about the construction timeline – which would you prefer?

I'd prefer quicker construction, even if it means no bridge access during that time.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

I'd like to keep driving routes clear. ,
Keeping public transport routes open is important.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#48

INCOMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Friday, February 02, 2024 3:14:14 PM
Last Modified: Friday, February 02, 2024 3:14:24 PM
Time Spent: 00:00:10
Language: English

Page 2: Travel Habits

Q1 Respondent skipped this question

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

Q2 I do it at least once a month.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3 Respondent skipped this question

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Q4 Respondent skipped this question

How about the construction timeline – which would you prefer?

Q5 Respondent skipped this question

Construction will bring some changes. What are your top three concerns from the list below?

Page 4: Follow-Up Question

Q6 Respondent skipped this question

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

Respondent skipped this question

We're curious how you found out about this project. Can you let us know?

#49

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Friday, February 02, 2024 7:37:36 PM
Last Modified: Friday, February 02, 2024 7:41:27 PM
Time Spent: 00:03:50
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I'm running errands.	Car
I'm visiting friends or family.	Car

Q2

I do it at least once a week.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be better for people walking. ,
We should improve it for people driving. ,
Keeping the bridge's look or its historical features is important.

Q4

How about the construction timeline – which would you prefer?

I'd prefer quicker construction, even if it means no bridge access during that time.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,
Let's keep noise and shaking to a minimum. ,
Let's finish the construction as quickly as possible.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I read about it on a neighborhood blog.

We're curious how you found out about this project. Can you let us know?

#50

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Thursday, February 08, 2024 7:49:18 PM
Last Modified: Thursday, February 08, 2024 7:52:25 PM
Time Spent: 00:03:07
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car
Another reason (please tell us). I drive from Bellevue to WS for chiropractic appointments regularly	

Q2 **I do it at least once a month.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3 **We should improve it for people driving.**

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Q4 **I don't mind a longer construction period if I can still use part of the bridge.**

How about the construction timeline – which would you prefer?

Q5 **I'd like to keep driving routes clear.**

Construction will bring some changes. What are your top three concerns from the list below?

Page 4: Follow-Up Question

Q6 **I'm not sure if there's another route.**

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7 **I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)**

We're curious how you found out about this project. Can you let us know?

#51

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Friday, February 16, 2024 3:59:15 PM
Last Modified: Friday, February 16, 2024 4:01:32 PM
Time Spent: 00:02:16
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I'm running errands.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it at least once a week.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

It should be better for people walking.

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Q4

I don't mind a longer construction period if I can still use part of the bridge.

How about the construction timeline – which would you prefer?

Q5

**Let's keep walking routes open and detour-free. ,
I'd like to keep driving routes clear.**

Construction will bring some changes. What are your top three concerns from the list below?

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I visited the City of Seattle/SDOT website.

We're curious how you found out about this project. Can you let us know?

#52

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 3:02:56 PM
Last Modified: Monday, February 19, 2024 3:08:15 PM
Time Spent: 00:05:18
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands.	Car, Bike
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Bike

Q2**I do it at least once a month.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
It should be better for people walking. ,
It should be more convenient for people who use public transport.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.**Q5**

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,
Keeping public transport routes open is important. ,
Let's keep noise and shaking to a minimum.

Page 4: Follow-Up Question

Q6

I'm not sure if there's another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#53

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 3:10:59 PM
Last Modified: Monday, February 19, 2024 3:15:17 PM
Time Spent: 00:04:17
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm visiting friends or family. **Car**

I'm doing it to go to fun activities. **Car**

Q2**I do it at least once a week.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3**It should be easier for people biking. ,**

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be better for people walking.**Q4****I don't mind a longer construction period if I can still use part of the bridge.**

How about the construction timeline – which would you prefer?

Q5**Keeping public transport routes open is important. ,**

Construction will bring some changes. What are your top three concerns from the list below?

Let's finish the construction as quickly as possible.

Page 4: Follow-Up Question

Q6

Respondent skipped this question

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#54

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 4:47:51 PM
Last Modified: Monday, February 19, 2024 4:48:58 PM
Time Spent: 00:01:07
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm visiting friends or family.

Car

Q2

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

I do it at least once a week.

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,

It should be better for people walking. ,

It should be more convenient for people who use public transport.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,

Let's minimize detours and closures for cyclists.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#55

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 4:49:07 PM
Last Modified: Monday, February 19, 2024 4:52:01 PM
Time Spent: 00:02:53
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car, Bus
I need it for my job, but not for getting to work (like freight, deliveries, rideshare, etc.)	Freight, Mobility Device/Walking, Bike
I'm running errands.	Car
I'm visiting friends or family.	Car, Bus
I'm doing it to go to fun activities.	Car, Bus

Q2 **I do it at least once a week.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?	It should be easier for people biking. , It should be better for people walking. , We should improve it for people driving. , It should be more convenient for people who use public transport. , Keeping the bridge's look or its historical features is important.
--	---

Q4

How about the construction timeline – which would you prefer?	I don't mind a longer construction period if I can still use part of the bridge.
---	---

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,
Let's minimize detours and closures for cyclists. ,
I'd like to keep driving routes clear. ,
Keeping public transport routes open is important. ,
Let's keep noise and shaking to a minimum.

Page 4: Follow-Up Question

Q6

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Yes, I have another route.

Q7

We're curious how you found out about this project. Can you let us know?

I received a letter or email from the City of Seattle/SDOT.

#56

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 4:49:55 PM
Last Modified: Monday, February 19, 2024 4:53:32 PM
Time Spent: 00:03:36
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands. **Car**
 I'm doing it to go to fun activities. **Car**
 Another reason (please tell us).
 Medical facilities

Q2**I do it at least once a week.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3**Respondent skipped this question**

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Q4**I don't mind a longer construction period if I can still use part of the bridge.**

How about the construction timeline – which would you prefer?

Q5

Let's keep walking routes open and detour-free. ,
I'd like to keep driving routes clear. ,
Keeping public transport routes open is important.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#57

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 4:57:50 PM
Last Modified: Monday, February 19, 2024 4:59:37 PM
Time Spent: 00:01:46
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.

Car

I'm running errands.

Bus, Bike

I'm visiting friends or family.

Car, Mobility Device/Walking, Bike

I'm doing it to go to fun activities.

Mobility Device/Walking, Bike

Q2

I do it at least once a week.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,

It should be better for people walking. ,

It should be more convenient for people who use public transport.

Q4

How about the construction timeline – which would you prefer?

I'd prefer quicker construction, even if it means no bridge access during that time.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,

Keeping public transport routes open is important. ,

Let's finish the construction as quickly as possible.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I read about it on a neighborhood blog.

We're curious how you found out about this project. Can you let us know?

#58

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 4:57:45 PM
Last Modified: Monday, February 19, 2024 5:00:06 PM
Time Spent: 00:02:21
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it at least once a day.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be better for people walking. ,
We should improve it for people driving. ,
Keeping the bridge's look or its historical features is important.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

I'd like to keep driving routes clear. ,
Keeping public transport routes open is important.

Page 4: Follow-Up Question

Q6

No, I don't have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#59

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 4:58:58 PM
Last Modified: Monday, February 19, 2024 5:01:02 PM
Time Spent: 00:02:04
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands.

Car, Mobility Device/Walking, Bus, Bike

I'm visiting friends or family.

Car, Mobility Device/Walking, Bus, Bike

I'm doing it to go to fun activities.

Car, Mobility Device/Walking, Bus, Bike

Q2

I do it at least once a week.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,

It should be better for people walking. ,

It should be more convenient for people who use public transport.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Keeping public transport routes open is important. ,

Let's finish the construction as quickly as possible.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#60

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 4:59:51 PM
Last Modified: Monday, February 19, 2024 5:02:19 PM
Time Spent: 00:02:28
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Bus, Bike
I'm running errands.	Car, Bus, Bike
I'm visiting friends or family.	Car, Bus, Bike
I'm doing it to go to fun activities.	Car, Bus, Bike

Q2**I do it at least once a week.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
It should be better for people walking. ,
It should be more convenient for people who use public transport.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.**Q5**

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,
Let's minimize detours and closures for cyclists. ,
Keeping public transport routes open is important.

Page 4: Follow-Up Question

Q6

I'm not sure if there's another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#61

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 5:00:24 PM
Last Modified: Monday, February 19, 2024 5:02:26 PM
Time Spent: 00:02:02
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I'm running errands.	Car
I'm doing it to go to fun activities.	Car

Q2**I do it at least once a week.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
It should be better for people walking. ,
It should be more convenient for people who use public transport.

Q4

How about the construction timeline – which would you prefer?

I'd prefer quicker construction, even if it means no bridge access during that time.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Keeping public transport routes open is important. ,
Let's finish the construction as quickly as possible.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#62

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 5:07:44 PM
Last Modified: Monday, February 19, 2024 5:09:47 PM
Time Spent: 00:02:03
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Bike
I need it for my job, but not for getting to work (like freight, deliveries, rideshare, etc.)	Bike
I'm running errands.	Bike
I'm visiting friends or family.	Bike
I'm doing it to go to fun activities.	Bike

Q2**I do it at least once a day.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
It should be better for people walking. ,
It should be more convenient for people who use public transport.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,
Let's minimize detours and closures for cyclists. ,
Keeping public transport routes open is important.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#63

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 5:07:49 PM
Last Modified: Monday, February 19, 2024 5:10:21 PM
Time Spent: 00:02:31
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Bus, Bike
I need it for my job, but not for getting to work (like freight, deliveries, rideshare, etc.)	Bus, Bike
I'm running errands.	Bus, Bike
I'm visiting friends or family.	Bus, Bike
I'm doing it to go to fun activities.	Bus, Bike

Q2 **I do it at least once a month.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3	It should be easier for people biking. ,
Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?	It should be better for people walking. ,
	It should be more convenient for people who use public transport.

Q4	I'd prefer quicker construction, even if it means no bridge access during that time.
How about the construction timeline – which would you prefer?	

Q5	Let's keep walking routes open and detour-free. ,
Construction will bring some changes. What are your top three concerns from the list below?	Let's minimize detours and closures for cyclists. ,
	Keeping public transport routes open is important.

Page 4: Follow-Up Question

Q6 **I'm not sure if there's another route.**

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7 **I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)**

We're curious how you found out about this project. Can you let us know?

#64

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 5:14:52 PM
Last Modified: Monday, February 19, 2024 5:16:52 PM
Time Spent: 00:02:00
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2**I do it at least once a week.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3**It should be better for people walking.**

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Q4**I'd prefer quicker construction, even if it means no bridge access during that time.**

How about the construction timeline – which would you prefer?

Q5**Let's keep walking routes open and detour-free. ,
Let's finish the construction as quickly as possible.**

Construction will bring some changes. What are your top three concerns from the list below?

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#65

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 5:10:54 PM
Last Modified: Monday, February 19, 2024 5:17:39 PM
Time Spent: 00:06:44
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.

Car, Mobility Device/Walking

I'm running errands.

Car, Mobility Device/Walking

I'm doing it to go to fun activities.

Car, Mobility Device/Walking

Q2

I do it less than once a month.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

It should be better for people walking. ,

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Keeping the bridge's look or its historical features is important.

Q4

I don't mind a longer construction period if I can still use part of the bridge.

How about the construction timeline – which would you prefer?

Q5

I'd like to keep driving routes clear. ,

Construction will bring some changes. What are your top three concerns from the list below?

Something else (please tell us more). :
 expense. I live in Alki. I'm disabled and on SS. A long detour would make Lyft rides very expensive. I'm not well enough to use water taxi all the time

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I read about it on a neighborhood blog.

We're curious how you found out about this project. Can you let us know?

#66

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 5:16:31 PM
Last Modified: Monday, February 19, 2024 5:20:06 PM
Time Spent: 00:03:35
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2**I do it at least once a week.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be better for people walking. ,
We should improve it for people driving. ,
It should be more convenient for people who use public transport.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.**Q5**

Construction will bring some changes. What are your top three concerns from the list below?

Keeping public transport routes open is important. ,
Let's finish the construction as quickly as possible.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#67

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 5:24:37 PM
Last Modified: Monday, February 19, 2024 5:26:24 PM
Time Spent: 00:01:46
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2**I do it at least once a day.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3**We should improve it for people driving. ,**

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should accommodate freight better.**Q4****I'd prefer quicker construction, even if it means no bridge access during that time.**

How about the construction timeline – which would you prefer?

Q5**I'd like to keep driving routes clear. ,**

Construction will bring some changes. What are your top three concerns from the list below?

I'm concerned about freight traffic detours or closures. ,**Let's finish the construction as quickly as possible.**

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#68

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 5:27:42 PM
Last Modified: Monday, February 19, 2024 5:30:20 PM
Time Spent: 00:02:38
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car, Bus
I'm running errands.	Car, Bus
I'm visiting friends or family.	Car, Bike
I'm doing it to go to fun activities.	Car, Bike

Q2**I do it at least once a month.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
It should be better for people walking. ,
Keeping the bridge's look or its historical features is important.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Keeping public transport routes open is important. ,
I'm concerned about freight traffic detours or closures. ,
I'm worried about the impact on property nearby.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#69

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 5:31:28 PM
Last Modified: Monday, February 19, 2024 5:32:53 PM
Time Spent: 00:01:25
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands. **Car**

I'm doing it to go to fun activities. **Car**

Q2**I do it at least once a month.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,**It should be better for people walking. ,****We should improve it for people driving.****Q4**

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.**Q5**

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,**Let's minimize detours and closures for cyclists. ,****I'd like to keep driving routes clear.**

Page 4: Follow-Up Question

Q6

I'm not sure if there's another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I read about it on a neighborhood blog.

We're curious how you found out about this project. Can you let us know?

#70

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 5:34:14 PM
Last Modified: Monday, February 19, 2024 5:36:12 PM
Time Spent: 00:01:57
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I need it for my job, but not for getting to work (like freight, deliveries, rideshare, etc.)	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2**I do it at least once a week.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3**It should be easier for people biking. ,**

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

We should improve it for people driving.**Q4****I'd prefer quicker construction, even if it means no bridge access during that time.**

How about the construction timeline – which would you prefer?

Q5**Keeping public transport routes open is important. ,**

Construction will bring some changes. What are your top three concerns from the list below?

Let's finish the construction as quickly as possible.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#71

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 5:39:00 PM
Last Modified: Monday, February 19, 2024 5:40:42 PM
Time Spent: 00:01:41
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm doing it to go to fun activities.

Bus, Bike

Q2

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

I do it less than once a month.

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,

It should be better for people walking. ,

It should be more convenient for people who use public transport.

Q4

How about the construction timeline – which would you prefer?

I'd prefer quicker construction, even if it means no bridge access during that time.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Keeping public transport routes open is important. ,

Let's finish the construction as quickly as possible.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#72

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 6:08:48 PM
Last Modified: Monday, February 19, 2024 6:11:03 PM
Time Spent: 00:02:15
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.

Car, Mobility Device/Walking, Bus, Bike

I'm running errands.

Car, Mobility Device/Walking, Bus, Bike

I'm visiting friends or family.

Car, Mobility Device/Walking, Bus, Bike

I'm doing it to go to fun activities.

Car, Mobility Device/Walking, Bus, Bike

Q2

I do it at least once a week.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,

It should be better for people walking. ,

It should be more convenient for people who use public transport.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,

Let's minimize detours and closures for cyclists. ,

Keeping public transport routes open is important.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#73

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 6:17:35 PM
Last Modified: Monday, February 19, 2024 6:20:01 PM
Time Spent: 00:02:26
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it at least once a week.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
It should be better for people walking. ,
Keeping the bridge's look or its historical features is important.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,
Let's minimize detours and closures for cyclists. ,
I'm concerned about freight traffic detours or closures.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#74

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 6:32:33 PM
Last Modified: Monday, February 19, 2024 6:35:42 PM
Time Spent: 00:03:09
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car, Bus
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car, Mobility Device/Walking, Bus

Q2**I do it at least once a day.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3**Keeping the bridge's look or its historical features is important.**

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Q4**I don't mind a longer construction period if I can still use part of the bridge.**

How about the construction timeline – which would you prefer?

Q5

I'd like to keep driving routes clear. ,
Keeping public transport routes open is important. ,
Let's finish the construction as quickly as possible.

Construction will bring some changes. What are your top three concerns from the list below?

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#75

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 7:02:54 PM
Last Modified: Monday, February 19, 2024 7:05:45 PM
Time Spent: 00:02:51
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2**I do it at least once a day.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be better for people walking. ,
We should improve it for people driving. ,
Keeping the bridge's look or its historical features is important.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

I'd like to keep driving routes clear. ,
Let's finish the construction as quickly as possible.

Page 4: Follow-Up Question

Q6

I'm not sure if there's another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

A friend, neighbor, or family member told me.

We're curious how you found out about this project. Can you let us know?

#76

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 7:14:30 PM
Last Modified: Monday, February 19, 2024 7:16:56 PM
Time Spent: 00:02:26
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2**I do it at least once a month.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3**We should improve it for people driving. ,**

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should accommodate freight better.**Q4****I don't mind a longer construction period if I can still use part of the bridge.**

How about the construction timeline – which would you prefer?

Q5**I'd like to keep driving routes clear. ,**

Construction will bring some changes. What are your top three concerns from the list below?

Let's finish the construction as quickly as possible.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#77

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 7:36:15 PM
Last Modified: Monday, February 19, 2024 7:39:08 PM
Time Spent: 00:02:53
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.

Car, Mobility Device/Walking, Bus

I'm running errands.

Car, Mobility Device/Walking

I'm visiting friends or family.

Car, Bus

I'm doing it to go to fun activities.

Car, Mobility Device/Walking

Q2

I do it at least once a day.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,

It should be better for people walking. ,

Keeping the bridge's look or its historical features is important.

Q4

How about the construction timeline – which would you prefer?

I'd prefer quicker construction, even if it means no bridge access during that time.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,

Keeping public transport routes open is important. ,

I'm worried about the impact on property nearby.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#78

INCOMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 7:53:48 PM
Last Modified: Monday, February 19, 2024 7:55:48 PM
Time Spent: 00:01:59
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I need it for my job, but not for getting to work (like freight, deliveries, rideshare, etc.)	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it at least once a week.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
It should be better for people walking. ,
We should improve it for people driving. ,
It should accommodate freight better.

Q4

How about the construction timeline – which would you prefer?

I'd prefer quicker construction, even if it means no bridge access during that time.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,
I'd like to keep driving routes clear. ,
Keeping public transport routes open is important. ,
Let's finish the construction as quickly as possible.

Page 4: Follow-Up Question

Q6

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Respondent skipped this question

Q7

We're curious how you found out about this project. Can you let us know?

Respondent skipped this question

#79

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 8:51:55 PM
Last Modified: Monday, February 19, 2024 8:54:58 PM
Time Spent: 00:03:03
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car, Bus

Q2**I do it at least once a day.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3**We should improve it for people driving.**

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Q4**I don't mind a longer construction period if I can still use part of the bridge.**

How about the construction timeline – which would you prefer?

Q5

Let's keep walking routes open and detour-free. ,
I'd like to keep driving routes clear. ,
Keeping public transport routes open is important.

Construction will bring some changes. What are your top three concerns from the list below?

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#80

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 9:33:58 PM
Last Modified: Monday, February 19, 2024 9:36:39 PM
Time Spent: 00:02:40
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car, Bike
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car, Mobility Device/Walking, Bike

Q2**I do it at least once a week.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3**We should improve it for people driving.**

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Q4**I don't mind a longer construction period if I can still use part of the bridge.**

How about the construction timeline – which would you prefer?

Q5

I'd like to keep driving routes clear. ,
Keeping public transport routes open is important. ,
I'm concerned about freight traffic detours or closures.

Construction will bring some changes. What are your top three concerns from the list below?

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

A friend, neighbor, or family member told me.

We're curious how you found out about this project. Can you let us know?

#81

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 10:20:26 PM
Last Modified: Monday, February 19, 2024 10:26:13 PM
Time Spent: 00:05:47
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Bus, Bike
I need it for my job, but not for getting to work (like freight, deliveries, rideshare, etc.)	Car
I'm running errands.	Car, Bus
I'm visiting friends or family.	Car, Bus, Bike
I'm doing it to go to fun activities.	Car, Bus, Bike

Q2

I do it at least once a day.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
It should be better for people walking. ,
It should be more convenient for people who use public transport.

,
 Something else (please tell us more). :

It should deprioritize drivers. We need to encourage less people to drive in this city- it makes riding the bus take forever and not sustainable.

Q4

How about the construction timeline – which would you prefer?

I'd prefer quicker construction, even if it means no bridge access during that time.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,
Let's minimize detours and closures for cyclists. ,
Keeping public transport routes open is important.

Page 4: Follow-Up Question

Q6

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Yes, I have another route.

Q7

We're curious how you found out about this project. Can you let us know?

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

#82

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, February 19, 2024 10:22:37 PM
Last Modified: Monday, February 19, 2024 10:35:09 PM
Time Spent: 00:12:32
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it at least once a day.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be better for people walking. ,
We should improve it for people driving. ,
Keeping the bridge's look or its historical features is important.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

I'd like to keep driving routes clear. ,
Keeping public transport routes open is important. ,
Let's finish the construction as quickly as possible.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#83

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Tuesday, February 20, 2024 12:03:39 AM
Last Modified: Tuesday, February 20, 2024 12:05:07 AM
Time Spent: 00:01:27
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I need it for my job, but not for getting to work (like freight, deliveries, rideshare, etc.)	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it at least once a day.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

We should improve it for people driving.

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Q4

I don't mind a longer construction period if I can still use part of the bridge.

How about the construction timeline – which would you prefer?

Q5

I'd like to keep driving routes clear. ,

Construction will bring some changes. What are your top three concerns from the list below?

Let's finish the construction as quickly as possible.

Page 4: Follow-Up Question

Q6

No, I don't have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I read about it on a neighborhood blog.

We're curious how you found out about this project. Can you let us know?

#84

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Tuesday, February 20, 2024 12:04:09 AM
Last Modified: Tuesday, February 20, 2024 12:07:31 AM
Time Spent: 00:03:21
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car, Bus, Bike
I'm running errands.	Car, Bus
I'm visiting friends or family.	Car, Bus, Bike
I'm doing it to go to fun activities.	Car, Bus, Bike

Q2**I do it at least once a month.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3**It should be easier for people biking. ,**

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be better for people walking.**Q4****I don't mind a longer construction period if I can still use part of the bridge.**

How about the construction timeline – which would you prefer?

Q5**Let's minimize detours and closures for cyclists. ,**

Construction will bring some changes. What are your top three concerns from the list below?

Keeping public transport routes open is important.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#85

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Tuesday, February 20, 2024 12:57:56 AM
Last Modified: Tuesday, February 20, 2024 12:59:39 AM
Time Spent: 00:01:42
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands.

Car, Mobility Device/Walking

I'm doing it to go to fun activities.

Car, Mobility Device/Walking

Q2

I do it at least once a week.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

It should be better for people walking. ,

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be more convenient for people who use public transport.

Q4

I'd prefer quicker construction, even if it means no bridge access during that time.

How about the construction timeline – which would you prefer?

Q5

Let's keep walking routes open and detour-free. ,

Construction will bring some changes. What are your top three concerns from the list below?

Keeping public transport routes open is important.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#86

INCOMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Tuesday, February 20, 2024 5:14:14 AM
Last Modified: Tuesday, February 20, 2024 5:15:04 AM
Time Spent: 00:00:50
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Bus
I need it for my job, but not for getting to work (like freight, deliveries, rideshare, etc.)	Bus
I'm running errands.	Bus
I'm visiting friends or family.	Bus
I'm doing it to go to fun activities.	Bus, Bike

Q2

I do it at least once a week.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Respondent skipped this question

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Q4

Respondent skipped this question

How about the construction timeline – which would you prefer?

Q5

Respondent skipped this question

Construction will bring some changes. What are your top three concerns from the list below?

Page 4: Follow-Up Question

Q6 **Respondent skipped this question**

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7 **Respondent skipped this question**

We're curious how you found out about this project. Can you let us know?

#87

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Tuesday, February 20, 2024 7:54:45 AM
Last Modified: Tuesday, February 20, 2024 7:56:53 AM
Time Spent: 00:02:08
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it at least once a day.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Keeping the bridge's look or its historical features is important.

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

Q4

I don't mind a longer construction period if I can still use part of the bridge.

How about the construction timeline – which would you prefer?

Q5

Let's keep walking routes open and detour-free. ,

Construction will bring some changes. What are your top three concerns from the list below?

Keeping public transport routes open is important.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#88

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Tuesday, February 20, 2024 8:29:10 AM
Last Modified: Tuesday, February 20, 2024 8:36:44 AM
Time Spent: 00:07:33
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it at least once a week.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
It should be better for people walking. ,
We should improve it for people driving. ,
It should be more convenient for people who use public transport.
,
Keeping the bridge's look or its historical features is important.
,
Something else (please tell us more). :
Really all of the above. I dont use it for biking or walking because it is to far from where i live but i do believe our streets should be multi purpose.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,

Let's minimize detours and closures for cyclists. ,

Keeping public transport routes open is important. ,

Something else (please tell us more). :

I only drive this way but i can go another way easily pedestrians and bikes cant.

Page 4: Follow-Up Question

Q6

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Yes, I have another route.

Q7

We're curious how you found out about this project. Can you let us know?

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

#89

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Tuesday, February 20, 2024 10:47:18 AM
Last Modified: Tuesday, February 20, 2024 10:50:38 AM
Time Spent: 00:03:20
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I'm running errands.	Car, Mobility Device/Walking
I'm visiting friends or family.	Car, Mobility Device/Walking
I'm doing it to go to fun activities.	Car, Mobility Device/Walking
Another reason (please tell us). Exercise	

Q2**I do it at least once a week.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be better for people walking. ,
We should improve it for people driving. ,
Keeping the bridge's look or its historical features is important.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

I'd like to keep driving routes clear. ,
I'm concerned about freight traffic detours or closures.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

A friend, neighbor, or family member told me.

We're curious how you found out about this project. Can you let us know?

#90

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Tuesday, February 20, 2024 11:51:03 AM
Last Modified: Tuesday, February 20, 2024 11:55:02 AM
Time Spent: 00:03:59
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands. **Car**

I'm doing it to go to fun activities. **Car**

Q2

I do it at least once a week.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Something else (please tell us more). :

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It's fine the way it is

Q4

I don't mind a longer construction period if I can still use part of the bridge.

How about the construction timeline – which would you prefer?

Q5

I'd like to keep driving routes clear.

Construction will bring some changes. What are your top three concerns from the list below?

Page 4: Follow-Up Question

Q6

No, I don't have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#91

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Tuesday, February 20, 2024 9:07:18 PM
Last Modified: Tuesday, February 20, 2024 9:08:46 PM
Time Spent: 00:01:27
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm doing it to go to fun activities.

Mobility Device/Walking

Q2

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

I do it less than once a month.

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be better for people walking. ,

It should be more convenient for people who use public transport.

,

Keeping the bridge's look or its historical features is important.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,

Let's minimize detours and closures for cyclists. ,

Keeping public transport routes open is important.

Page 4: Follow-Up Question

Q6

I'm not sure if there's another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#92

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Thursday, February 22, 2024 4:32:14 PM
Last Modified: Thursday, February 22, 2024 4:36:21 PM
Time Spent: 00:04:06
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2**I do it at least once a week.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
It should be better for people walking. ,
We should improve it for people driving.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,
I'd like to keep driving routes clear. ,
Let's finish the construction as quickly as possible.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?

#93

INCOMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Friday, February 23, 2024 1:50:25 PM
Last Modified: Friday, February 23, 2024 1:54:09 PM
Time Spent: 00:03:44
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car
I'm running errands.	Car
I'm visiting friends or family.	Car
I'm doing it to go to fun activities.	Car

Q2

I do it at least once a day.

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be better for people walking. ,
We should improve it for people driving. ,
It should be more convenient for people who use public transport.
 ,
Keeping the bridge's look or its historical features is important.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

- Let's keep walking routes open and detour-free. ,
- I'd like to keep driving routes clear. ,
- Keeping public transport routes open is important. ,
- Let's keep noise and shaking to a minimum. ,
- I'm worried about the impact on property nearby. ,
- Let's finish the construction as quickly as possible.

Page 4: Follow-Up Question

Q6

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Respondent skipped this question

Q7

We're curious how you found out about this project. Can you let us know?

Respondent skipped this question

#94

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, March 04, 2024 11:30:34 AM
Last Modified: Monday, March 04, 2024 11:32:33 AM
Time Spent: 00:01:59
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I go to and from work or school.	Car, Bus, Bike
I'm running errands.	Car, Bus, Bike
I'm visiting friends or family.	Car, Bus, Bike
I'm doing it to go to fun activities.	Car, Bus, Bike

Q2**I do it at least once a week.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,
It should be better for people walking. ,
It should be more convenient for people who use public transport.

Q4

How about the construction timeline – which would you prefer?

I don't mind a longer construction period if I can still use part of the bridge.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,
Let's minimize detours and closures for cyclists. ,
Keeping public transport routes open is important.

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

Other ways I found out (please share with us). :

We're curious how you found out about this project. Can you let us know?

Twitter

#95

COMPLETE

Collector: Admiral Way SW - Link (Web Link)
Started: Monday, March 04, 2024 3:22:05 PM
Last Modified: Monday, March 04, 2024 3:24:54 PM
Time Spent: 00:02:49
Language: English

Page 2: Travel Habits

Q1

Why do you use the Admiral Way SW Bridges and how do you use them? (you can choose more than one answer, for each type of travel)

I'm running errands. **Car, Bus**

I'm doing it to go to fun activities. **Car, Bus**

Q2**I do it at least once a week.**

How often do you travel over or under (along Fairmount Ave SW) the Admiral Way SW Bridges?

Page 3: Help Us Understand Your Preferences for the Admiral Way SW Bridge Project

Q3

Whenever we do replace the Admiral Way SW Bridges, we want to make sure we focus on what matters most to you. Could you tell us your top three priorities from the list below?

It should be easier for people biking. ,**It should be better for people walking. ,****It should be more convenient for people who use public transport.**

,

It should accommodate freight better. ,**Keeping the bridge's look or its historical features is important.**

Q4

How about the construction timeline – which would you prefer?

I'd prefer quicker construction, even if it means no bridge access during that time.

Q5

Construction will bring some changes. What are your top three concerns from the list below?

Let's keep walking routes open and detour-free. ,**Let's minimize detours and closures for cyclists. ,****Keeping public transport routes open is important.**

Page 4: Follow-Up Question

Q6

Yes, I have another route.

If you told us you want fewer detours or closures, we have a follow-up question. If the Admiral Way Bridges were not available, do you have another route?

Q7

I found out through social media (Twitter, Facebook, Instagram, Nextdoor, etc.)

We're curious how you found out about this project. Can you let us know?
